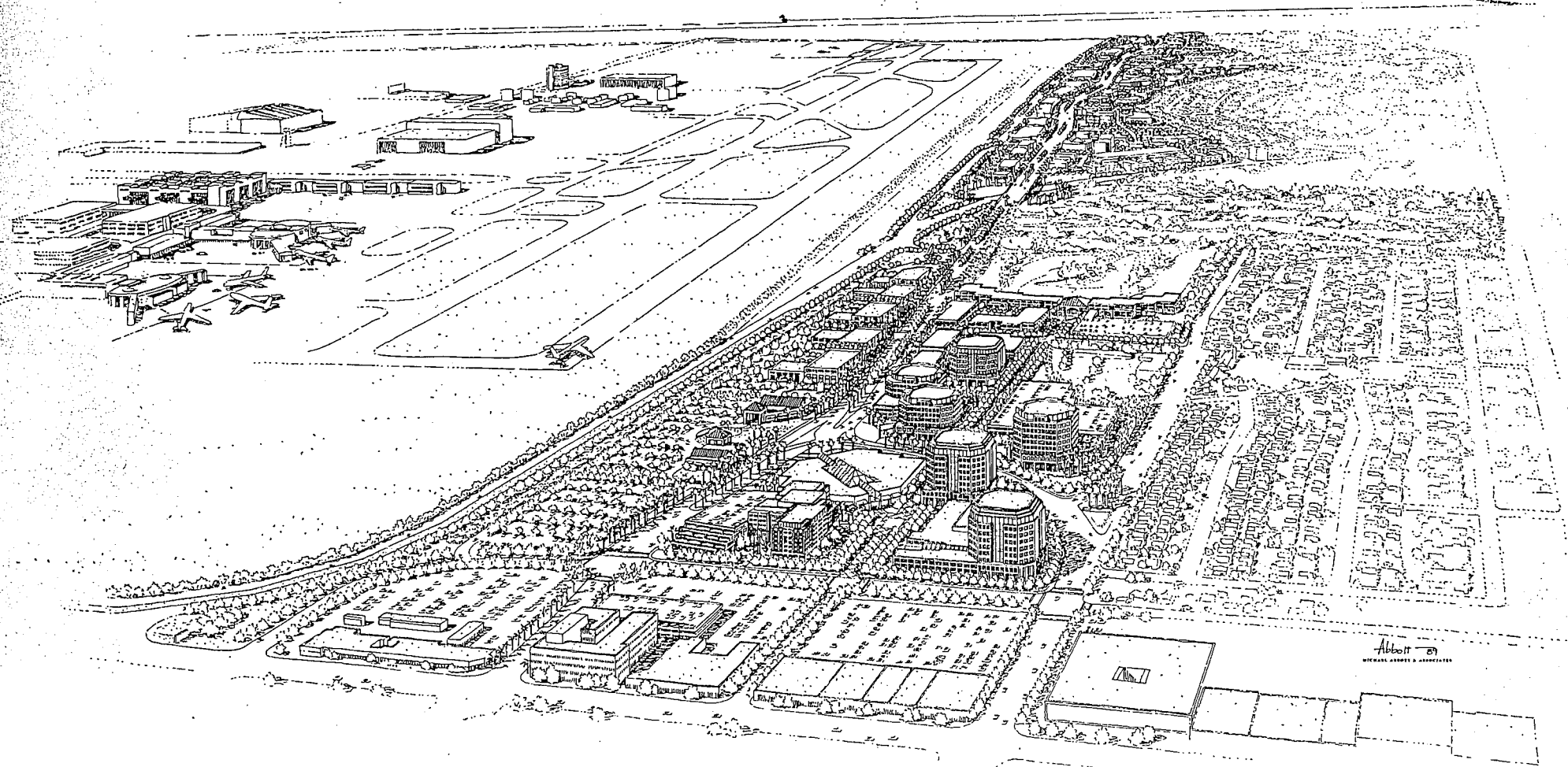


Design Plan and Development Guidelines

# ***LAX NORTHSIDE***



Abbott  
MICHAEL ABBOTT & ASSOCIATES

Design Plan and Development Guidelines

# ***LAX NORTHSIDE***

Prepared By:

**Albert C. Martin & Associates**

Takata Associates

Sussman / Prejza and Company, Inc.

Natelson / Levander / Whitney

Prepared For:

**The City of Los Angeles**

**Department of Airports**

April 20, 1989

# ***LAX NORTHSIDE***

## **Design Plan and Development Guidelines**

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# ***LAX NORTHSIDE***

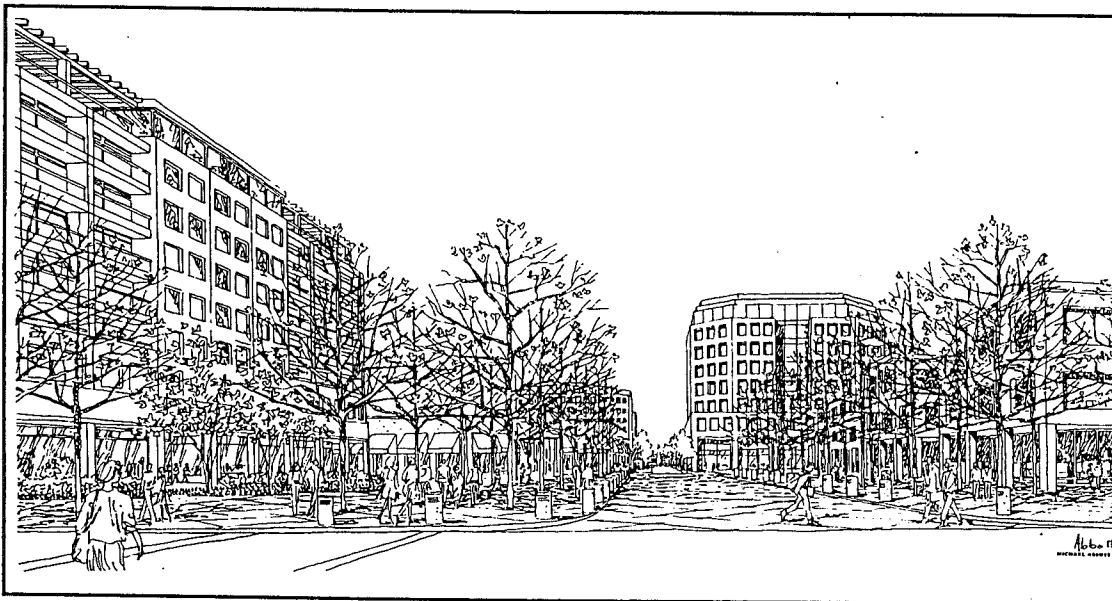
## **Design Plan and Development Guidelines**

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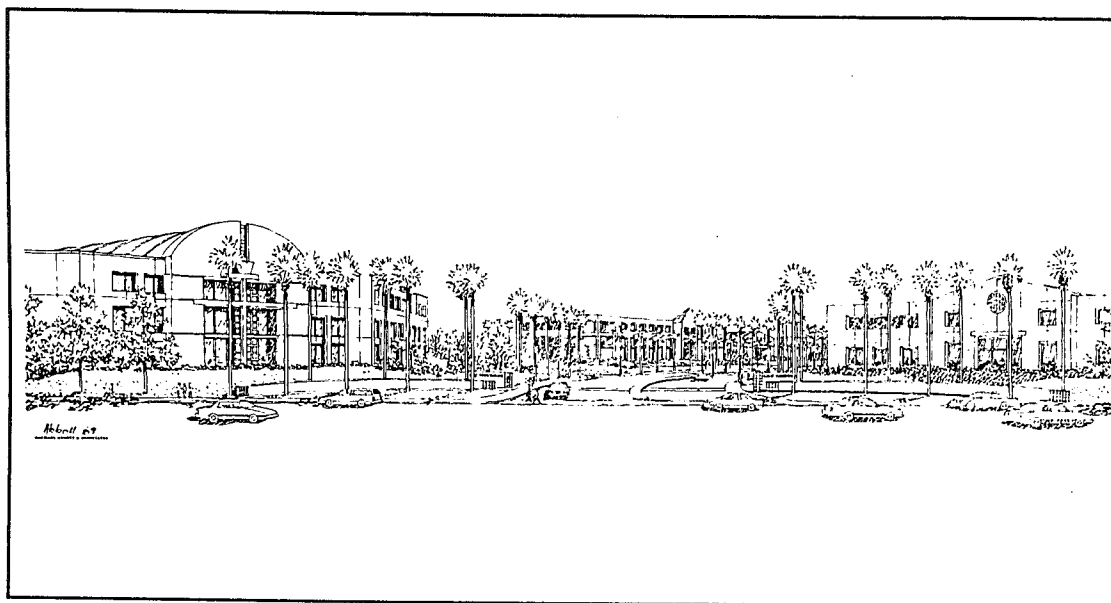


# 1.0 INTRODUCTION





*View of LAX Northside from Westchester*



*View of LAX Northside at Pershing Drive*

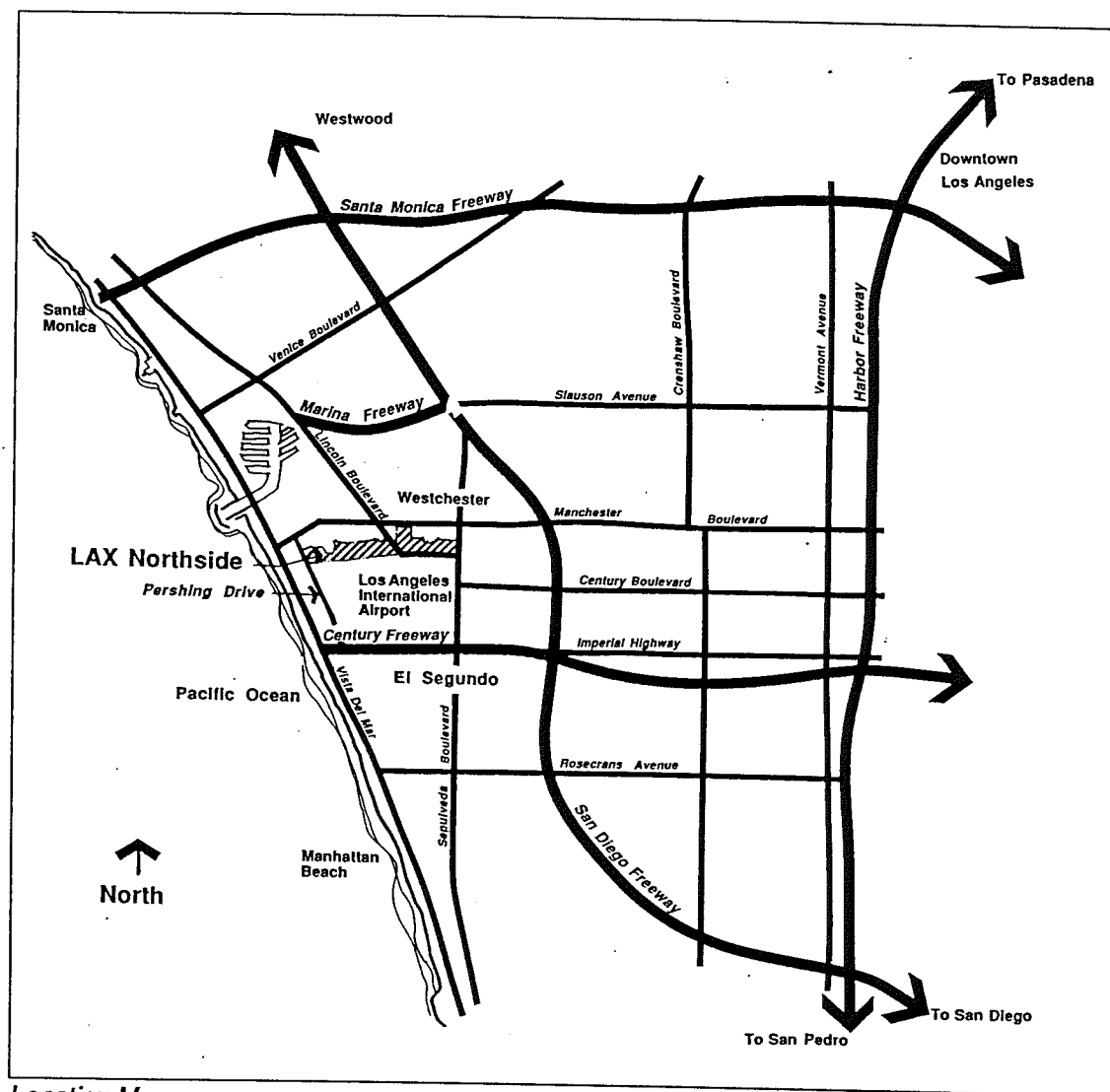
*LAX Northside* is a new business community and commercial center which will be built on City of Los Angeles Department of Airport (DOA) property north of the north runway at Los Angeles International Airport. The 350 acre development will consist of a balanced mix of office and research park, recreation, hotel, restaurant, and airport-related land uses along Westchester Parkway, a new, landscaped arterial highway being built across the property. At the center of *LAX Northside* lies a redesigned, refurbished 18 hole public golf course.

The site extends nearly two and a half miles from the Westchester business district to Pershing Drive. Formerly a residential area, the property was acquired by the DOA as a buffer between the airport and residential neighborhoods which continue north of the project.

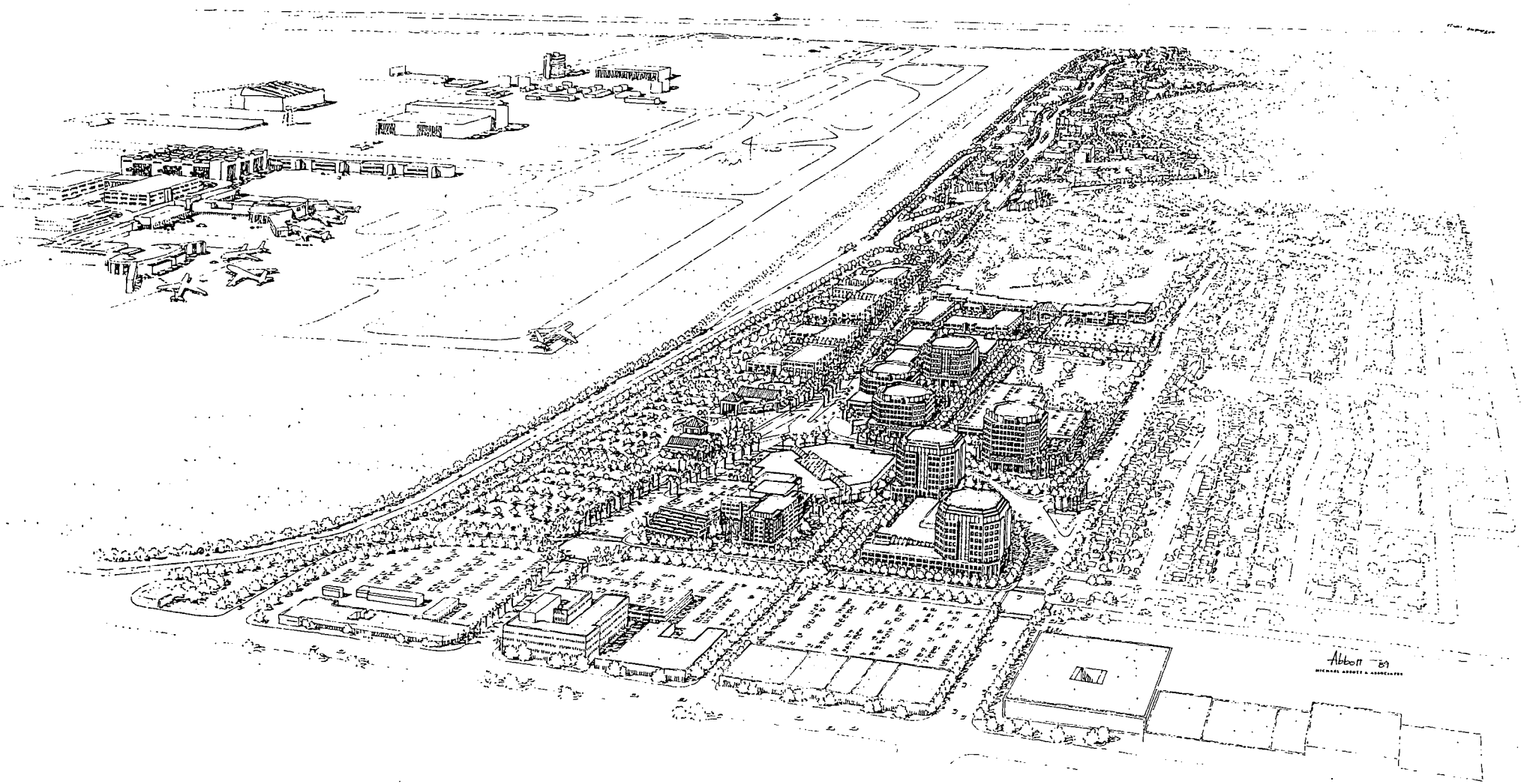
The DOA has determined that the property, which is not needed for airport operations, should be developed in a way which will be compatible with the adjoining neighborhoods, help restore vitality to the Westchester downtown business community and return the land to a productive use. The DOA conducted a series of public workshops and prepared market studies to determine the land uses which would be appropriate for this unique location. A tentative tract map, Zoning and Environmental Impact Report were approved for the site in 1984. At that time numerous Qualifying ("Q") conditions were applied as part of the approval process. One of the requirements was that a design plan and development guidelines would be prepared. This document will serve as those guidelines.

The design standards contained in these guidelines will be used by developers in preparing development plans for the site, and will also be used by the Department of Airports in reviewing proposals for the site. All proposals for construction within airport property, which includes *LAX Northside*, must be approved by the Department of Airports.

In addition to receiving DOA approval, and satisfying normal zoning and building department criteria, all projects on City property, which includes this site, must receive approval of the City of Los Angeles Cultural Affairs Commission. The FAA also reviews all projects in the vicinity of the airport.

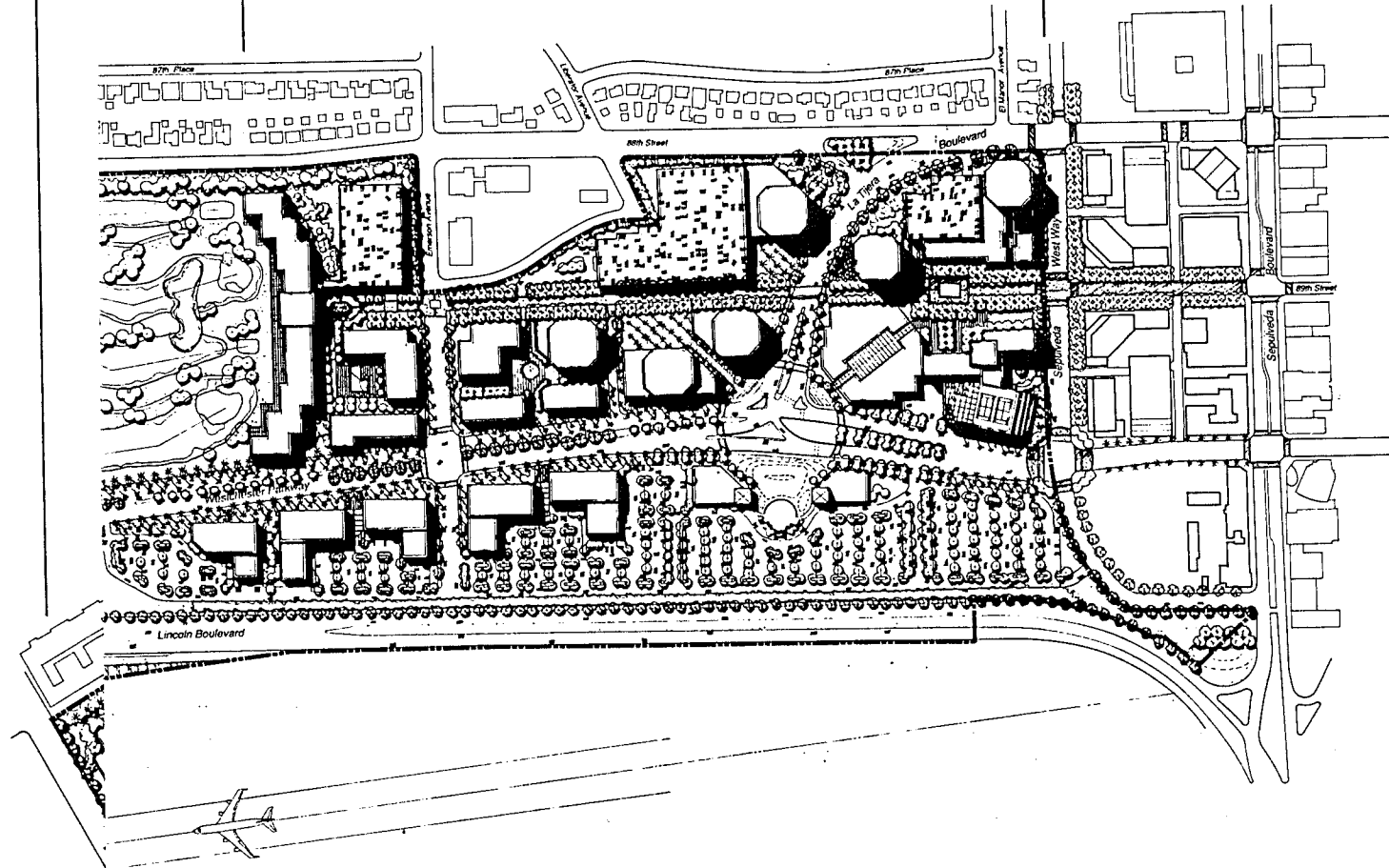


Location Map



Albion 201  
MICHAEL ADRIAN & ASSOCIATES

Commercial Center  
Office, Hotel, Retail, Restaurant



ILLUSTRATIVE PLAN  
LAX NORTHSIDE

Albert C. Martin and Associates  
Architects-Engineers-Interior Designers  
Landscape Architects & Planners  
Total Associates



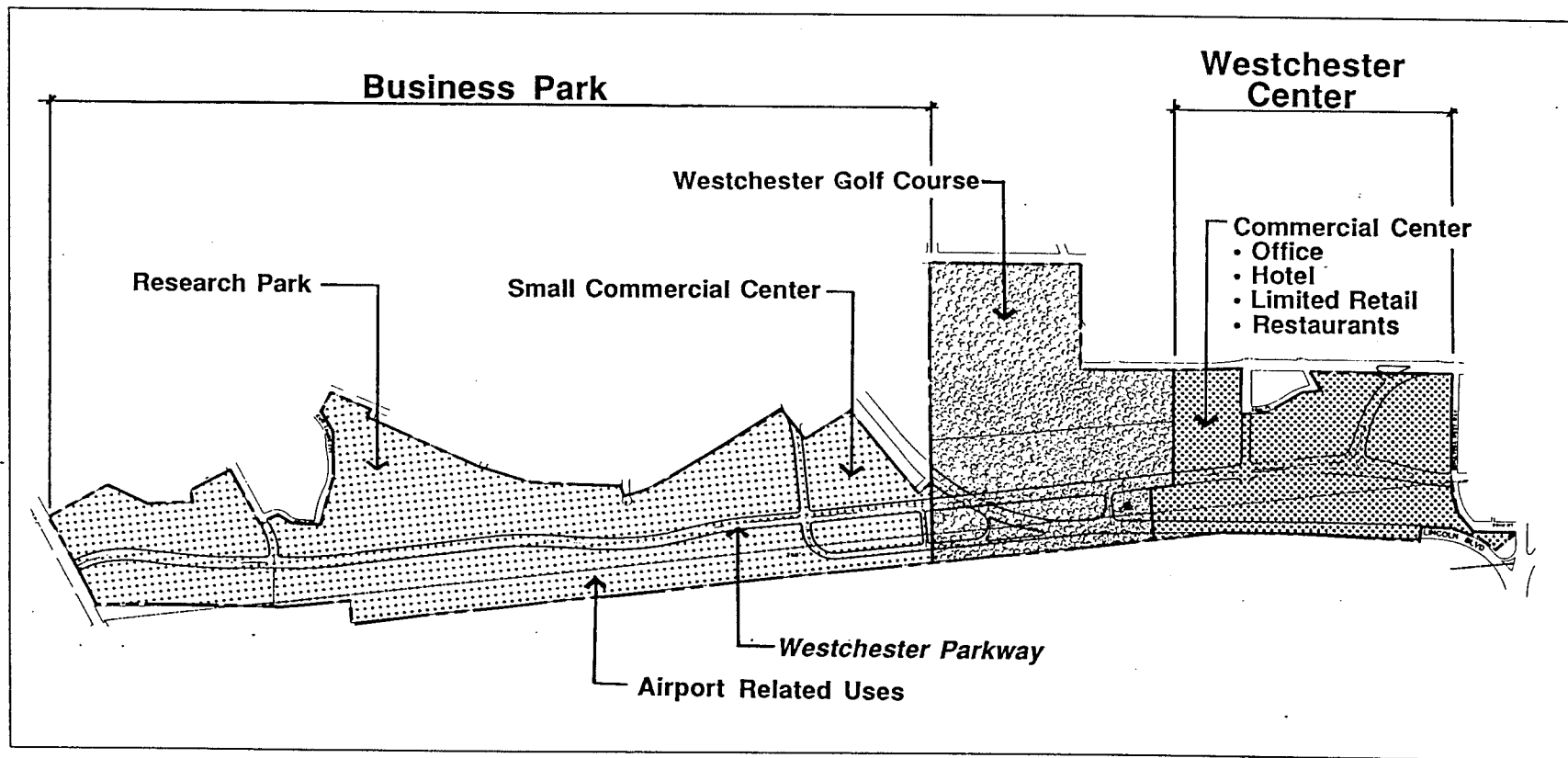
## 1.1 Purpose of the Guidelines

These guidelines have several basic purposes:

- To provide developers with a unifying theme and design handbook.
- To provide the Department of Airports with a basis for reviewing and coordinating project development plans.
- To help implement the zoning and tract conditions.
- To establish a high level of design quality, and a method for reviewing projects for conformance with that quality.
- To protect private investment, both existing and future, by assuring the compatibility between adjacent developments and avoiding future conflicts.
- To minimize delays in the design process.

Within these goals the guidelines are designed to provide maximum flexibility to respond to changing market requirements.

These guidelines do not replace the requirements of the "Q" conditions imposed as part of the zoning approval. These guidelines contain a summary of some of the conditions related to design and, in instances where there are overlapping concerns, reference is made within these guidelines to the zoning or tract conditions. However, for a full representation of the zoning and tract requirements, the zoning and tract conditions, as well as the City of Los Angeles Zoning Code, should be referred to directly. A copy of the Zoning and Tract Map conditions are contained in an appendix to this report. Wherever the Zoning Code is more restrictive than these guidelines, the Zoning code takes precedence.



*Development Zones*



## 1.2 Development Summary

*LAX Northside* consists of two major development areas:

***LAX Northside—Westchester Center*** in the eastern part of the site, and

***LAX Northside—Business Park*** in the western part of the site.

The Westchester Golf Course occupies the central portion of the site, between these two areas.

### Westchester Center

*Westchester Center* will be a mixed use urban center, consisting of office buildings and hotels, with a limited amount of supporting services, retail and restaurants. To reinforce their urban character, the buildings will have close relationships to each other and to the street. Pedestrian connections and the uses along them are important. Open spaces will be relatively small, enclosed or partially enclosed, and shielded from airport noise to the extent practical. *Westchester Center* will have moderate densities, and may be built to the maximum heights allowed by the FAA.

### Business Park

The *Business Park* will be a business and research park of relatively low density. *Westchester Parkway* will provide the frontage and identity for most of the buildings within the business park. Buildings along *Westchester Parkway* will be set back from the right of way to create a band of open space which will act as a linear park. Within the *Business Park* buildings will be distributed evenly, and most of the uses will be low, two-story research buildings containing a mix of office and light industrial spaces, some of which will be oriented toward airport-related uses.

*LAX Northside—Business Park* will also contain a small commercial center designed to serve the needs of the business park. This center may include a hotel.

### Summary Program

The zoning for the site describes the permitted land uses. The maximum buildable density of *LAX Northside* is defined not in terms of floor area ratios, but in terms of the traffic which the project will be allowed to generate.

Based on this limit, *Westchester Center* is projected to include, on development sites totalling 69 acres, development of:

• Low- and mid-rise office space	1,360,000 s.f.	
• Restaurant and retail services	100,000 s.f.	
• Hotel space	<u>650,000 s.f.</u>	
Total	2,110,000 s.f.	= 48.4 Ac

The *Business Park* is projected to include, on sites totalling 170 acres:

• Low-rise offices	220,000 s.f.	
• Research park	1,170,000 s.f.	
• Airport support facilities	<del>750,000 s.f.</del>	? 2,482,920
• Restaurant and retail services	30,000 s.f.	
• Hotel space	<u>220,000 s.f.</u>	
Total	<del>2,390,000 s.f.</del>	7

At buildout, development at *LAX Northside* will total approximately 4,500,000 s.f. The final program determination will be based on traffic generation, which will be monitored after the project achieves a floor area of 3,500,000 s.f.

### 1.3 Organization of the Guidelines

The Design Plan and Development Guidelines are comprised of several sections, each dealing with a separate topic. Following this introduction there is a description of the planning framework (Section 2) which defines the connections between this project and the surrounding community, and describes the general design features which will provide continuity within the project. The next section (Section 3) details the land uses and building density which will be permitted within each of the parcels of development.

This is followed by sections which describe the design guidelines for:

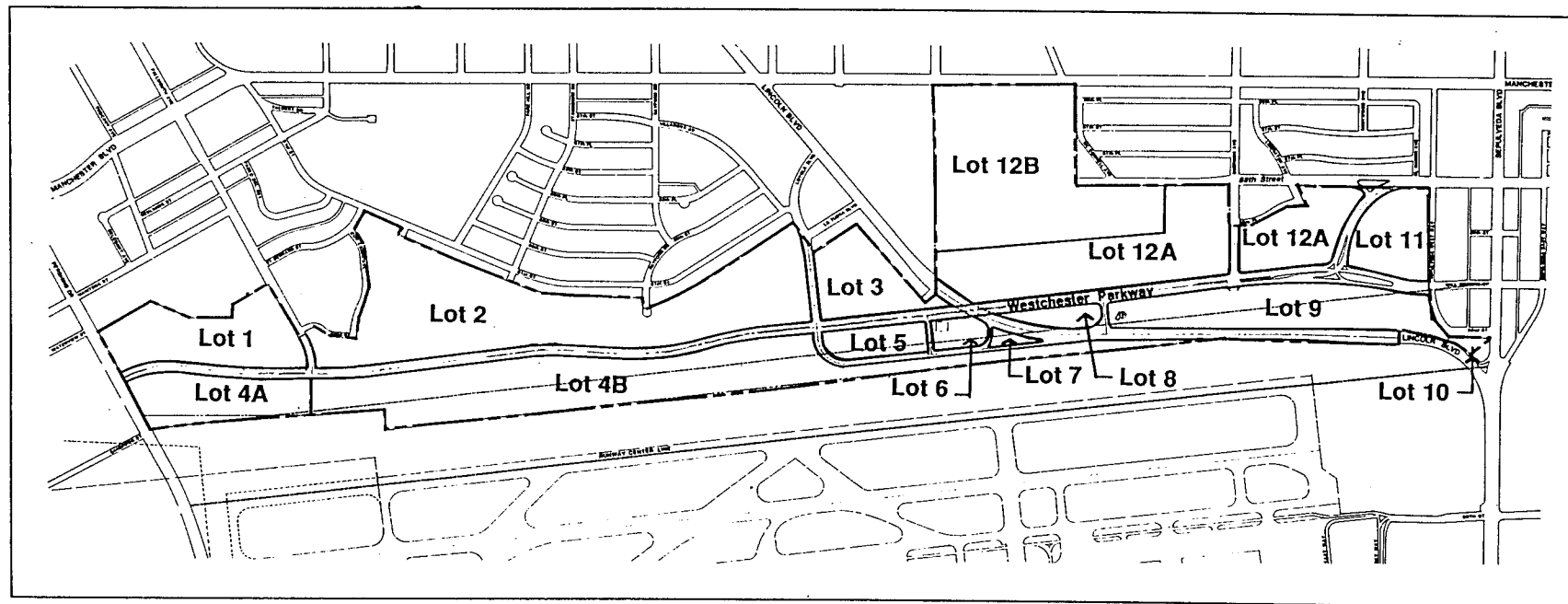
- site access
- architecture
- landscape
- lighting
- signing

Within each guideline section, there is a discussion of the background and issues relating to the topic, followed by a set of directive guidelines.

There are three types of guidelines, based on the area of application:

- General Guidelines apply to the entire project area.
- Westchester Center Guidelines apply to the areas east of Lincoln Boulevard.
- Business Park Guidelines apply to the areas west of Lincoln Boulevard.

In addition there are, in some cases, guidelines which apply to specific locations or uses within the project. The Tract Map for the project has divided the site into twelve legal parcels, numbered 1 through 12. These numbers will be used in referring to specific lots.

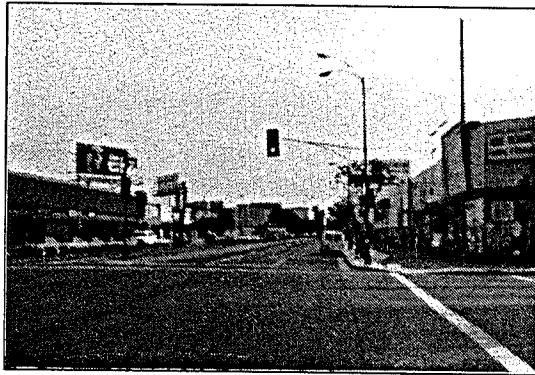


LAX Northside Lot Numbers

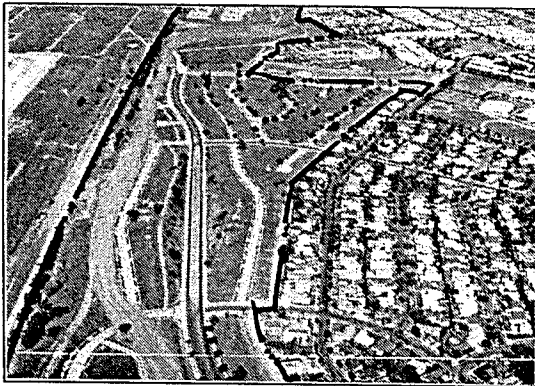
**2.0**

# **DEVELOPMENT FRAMEWORK**

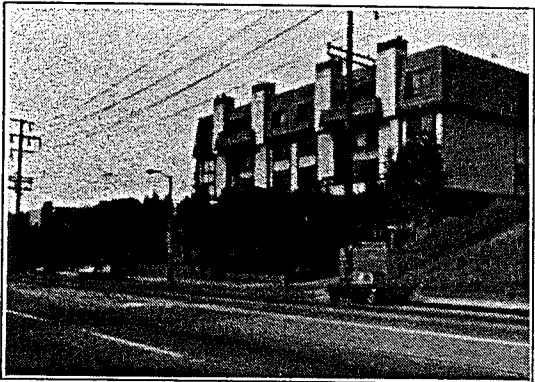




*Westchester Business District*



*Adjoining Residential Neighborhood*



*Pershing Drive*

## 2.1 Urban Context

*LAX Northside* is a linear development, approximately 2 1/3 miles in length, paralleling the north runway of Los Angeles International Airport. Each of the edges of the project will require a different response to assure compatibility of the new development with the existing urban fabric.

### Westchester Business District

The eastern edge of the project is contiguous with the existing Westchester Business District. This district has served as a small regional center, anchored by two department stores. It has deep shops fronting Sepulveda Boulevard, with large parking areas behind. Increasingly the retail buildings in Westchester have been converted to office uses, with a smaller core of retail consolidated in the blocks northeast of *LAX Northside*. One of the goals of *LAX Northside* is to become a catalyst for increased vitality in the Westchester Business District.

### Los Angeles International Airport

Along its entire southern edge *LAX Northside* adjoins the north runway of Los Angeles International Airport. For much of its common boundary, there is an open drainage channel which separates the runway from the site. There have been discussions about enclosing this channel; however at this point there are no plans to do so. The DOA may permit direct access from some parcels to the LAX internal

road system, if the channel is bridged, and security requirements can be met.

Building setback and height requirements based on FAA standards will strongly influence development along this edge. (See Section 2.3.)

### Pershing Drive

The western edge of *LAX Northside* is along Pershing Drive, a major highway which offers visibility into the project. The area across Pershing Drive from *LAX Northside* is currently undeveloped open space owned by the Department of Airports as a clear zone at the end of the runways. The land is proposed to be developed as golf courses. Views of and across this space are a valuable asset of the site.

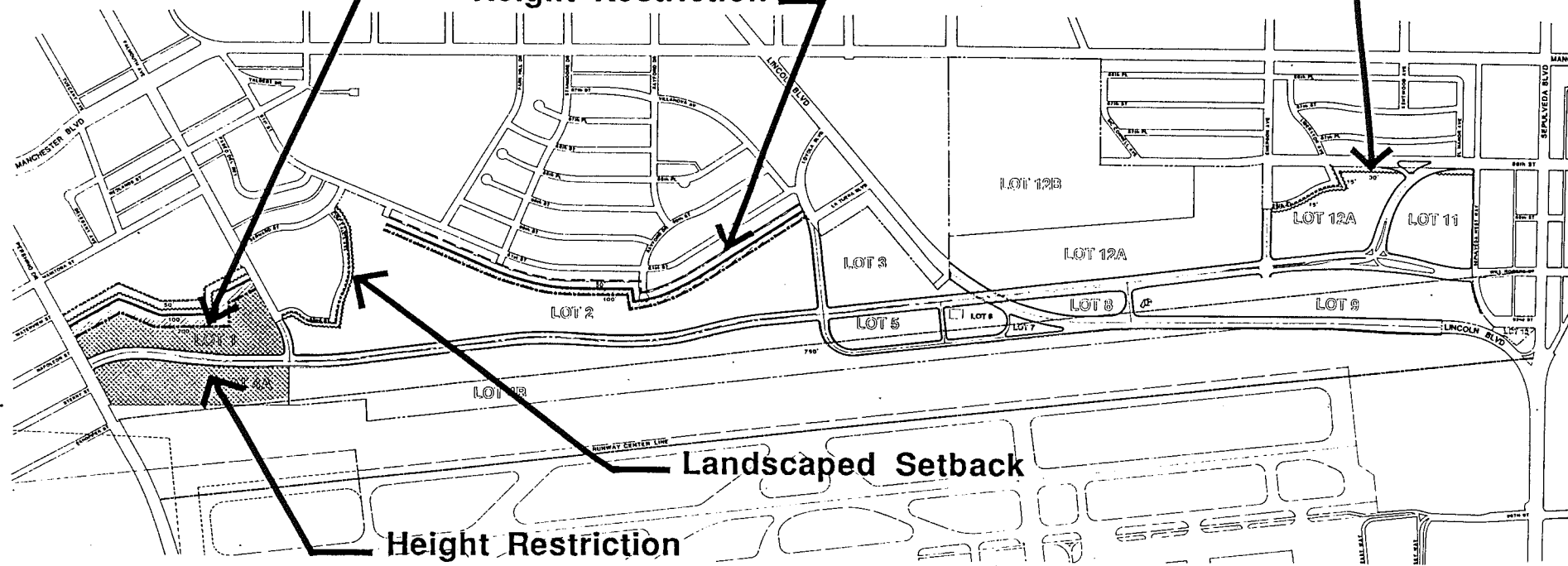
### Residential Neighborhoods

On the northern edge of the project lie several residential neighborhoods, comprised mostly of single family houses, but also containing two schools and some higher density condominiums. Buffers between *LAX Northside* and these neighborhoods are required by the "Q" Conditions. For most of its length, the buffer will serve as a physical and visual barrier between *LAX Northside* and the adjoining community, but in the westernmost area, the buffer must allow views across and through *LAX Northside*. The required buffers are described in the next section.







Landscape Setback,  
Building Setback,  
Height Restriction

Building Setback,  
Height Restriction

Landscaped Setback



## LEGEND

-  BUILDING HEIGHT RESTRICTION  
NOT TO EXCEED 3 STORIES  
OR 45' FROM FINISHED GRADE
-  BUILDING HEIGHT RESTRICTION  
NOT TO EXCEED 4 STORIES  
OR 55' FROM FINISHED GRADE
-  BUILDING SETBACK-NO  
STRUCTURES ALLOWED  
WITHIN SETBACK
-  LANDSCAPED BUFFER SETBACK
-  6' CHAIN LINK FENCE
-  8' MASONRY WALL

"Q" Condition Buffer Requirements

## 2.2 "Q" Conditions

The zoning adopted for the site in 1984 culminated a long process of community involvement. Public workshops had been conducted and well attended by neighboring residents and business people, in order to assure that the planning for the project reflected their concerns. Principal concerns of workshop participants included effective and attractive buffer systems between the development area and adjoining residential property, and a roadway system that would handle increased traffic load without intrusion into bordering residential neighborhoods. To meet these concerns, the plan calls for a variety of buffer systems, including fencing, shrubbery, trees, ground cover, walls, earth berms and landscaped building setbacks. These buffers were incorporated into the "Q" conditions adopted as part of the zoning.

Other issues addressed in the "Q" conditions are land use, allowable density, height, parking, hours of delivery, noise, access restrictions, security, and Transportation Systems Management. For a complete representation of the conditions, the full document should be referred to.

Many of the guidelines in this report exceed the "Q" conditions, and are additional requirements.

## 2.3 FAA Restrictions

The Federal Aviation Administration regulates the heights of structures in the vicinity of the airport. Parallel to the centerline of the north runway there is a clear zone of 500 feet, and then a transition slope of 7 to 1 (horizontal to vertical) which defines the maximum height of objects (buildings, landscaping, vehicles, etc.) relative to the elevation of the runway. This slope prevails until a height of 150 feet above the runway is reached. Buildings beyond this transition slope are limited to 150 feet in height, relative to the base elevation of the runway.

Also parallel to the runway is a building setback line 750 feet from the runway centerline. Between 500 feet and 750 feet, parking and landscaping may be permitted, if they do not penetrate the transition slope, but no structures are permitted.

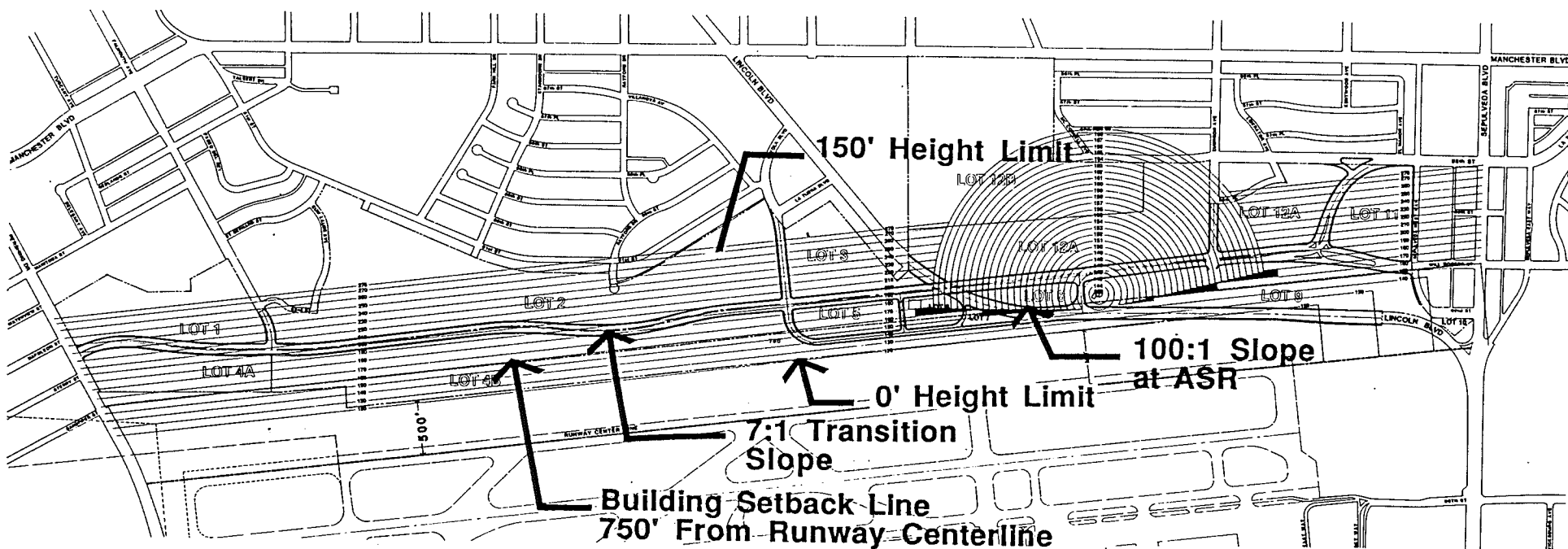
Heights are also controlled by the proximity to Airport Surveillance Radar (ASR) equipment located in Lot 9. From the elevation of this radar, there is an inverted cone which rises at a slope of 100 to 1 (horizontal to vertical) for a radius of 1500 feet from the center of the radar. Relocation of this radar is under consideration, and the FAA has determined, in a January 1988 report, that two alternate sites several hundred feet north and west of the current site would be acceptable. Of the two sites, the

location preferred by the FAA would be within the Westchester Golf Course, on the north side of Westchester Parkway. The preferred site is more distant from the runway and would allow the ASR to be located on a higher tower than currently. Because of both the increased height and the location, the associated height limitations would be less restrictive on potential development. If desirous of relocating the ASR, the developer of *Westchester Center* will be responsible for pursuing the issue with the FAA.




The FAA should be consulted before finalizing plans for Westchester Center to determine the feasibility of relocating the ASR.

The FAA has also expressed a desire to relocate ASDI radar which monitors ground traffic, and has selected a site within *LAX Northside* for its potential location. The site would be north of Westchester Parkway, west of Lincoln Boulevard. This radar could be located on the roof of the tallest building in the vicinity, and may have little impact on the heights of surrounding structures.

The FAA should be consulted before finalizing plans for the research park to determine the feasibility of locating ASDI equipment on the roof of a building within the project area.



## LEGEND

-  7:1 Height Restriction From Center Line North Runway
-  FAA Radar Requirements
-  Setback Line

FAA Height Restrictions



## 2.4 Regional Access

### Westchester Parkway

Westchester Parkway will be built as an arterial highway and will provide access to every development site within *LAX Northside*. Westchester Parkway will run the length of the site as the primary circulation, orientation, and open space feature for all the development within the project.

East of the project, Westchester Parkway extends through downtown Westchester, crosses LAX Parking Lot C, and continues as Arbor Vitae Street to the San Diego Freeway (Interstate 405). There are proposals to provide direct access to and from Arbor Vitae and the San Diego Freeway. Construction of this interchange depends on the provision of funding by CalTrans, the City of Los Angeles, and the City of Inglewood.

Within *LAX Northside*, Westchester Parkway will change from an urban streetscape to a suburban parkway as it passes from the Westchester Business District on the east toward the research park on the west. However, the landscaping, both within and along Westchester Parkway, will establish a common theme. These guidelines describe the character of the street, and help assure that each project, whether it be a single building or a group of buildings, will contribute to the design of *LAX Northside*.

### Light Rail

The Coastal Corridor Rail Transit Project is proposed to serve *LAX Northside*. The rail line is planned to extend from El Segundo on the south to Marina Del Rey on the north, connecting with the Norwalk-El Segundo /Rail Transit Project being constructed within the right-of-way of the Century Freeway (I-105). Future rail projects would extend this line into a network serving much of Los Angeles County, as mandated by Proposition A adopted by the voters in 1980. The Los Angeles County Transportation Commission is the lead agency responsible for planning the rail line.

Within *LAX Northside* the rail line is proposed to be built as an elevated structure along Westchester Parkway between Sepulveda Westway and Lincoln Boulevard. North of *LAX Northside*, the route would continue along the eastern edge of Lincoln Boulevard, going underground before Manchester Avenue, in order to maintain a gentle slope before surfacing at Playa Vista.

Two options for the line within *LAX Northside* have been identified by the LACTC: either within the median or along the south side of Westchester Parkway. The width of the right of way of Westchester Parkway has been designed to accommodate an elevated structure; however some redesign and modification of the street may be required as definitive

engineering plans for the light rail system are developed. If the south alignment is selected, additional right-of-way will be required.

Both options would provide a station along Westchester Parkway, between Sepulveda Westway and La Tijera. This station would serve the development within the Westchester Center, as well as existing and potential development within all of downtown Westchester. If the median alignment is adopted, the Westchester Station would be in the median of Westchester Parkway, between Sepulveda Westway and La Tijera Boulevard. If the south alignment is adopted, the Westchester Station would be opposite Sepulveda Westway. The LACTC desires a park-and-ride facility for 500 cars adjacent to this station. This park and ride facility should be located in Lot 9, which has very limited development potential due to the closeness to the runway and FAA restrictions. North of *LAX Northside* an aerial station on Lincoln Boulevard is proposed 1500 feet south of Manchester. This station would provide service to the eastern end of the Business Park.

The Coastal Corridor, although a high priority route, is in competition with other light rail segments for funding, and fiscal constraints may require that the construction proceed in phases. If so, the terminus of the first phase

would be either the LAX/Lot C Station or the Westchester Station. (Source: Draft Environmental Impact Report, January 1989, Coastal Corridor Rail Transit Project-Northern Segment).

A recent proposal, by the Councilwoman's office, suggests evaluation of an alternative route which would not run along Westchester Parkway and Lincoln Boulevard, but would instead run north along Sepulveda Boulevard. No plans for this alternative have been developed, although it is under consideration for further study.

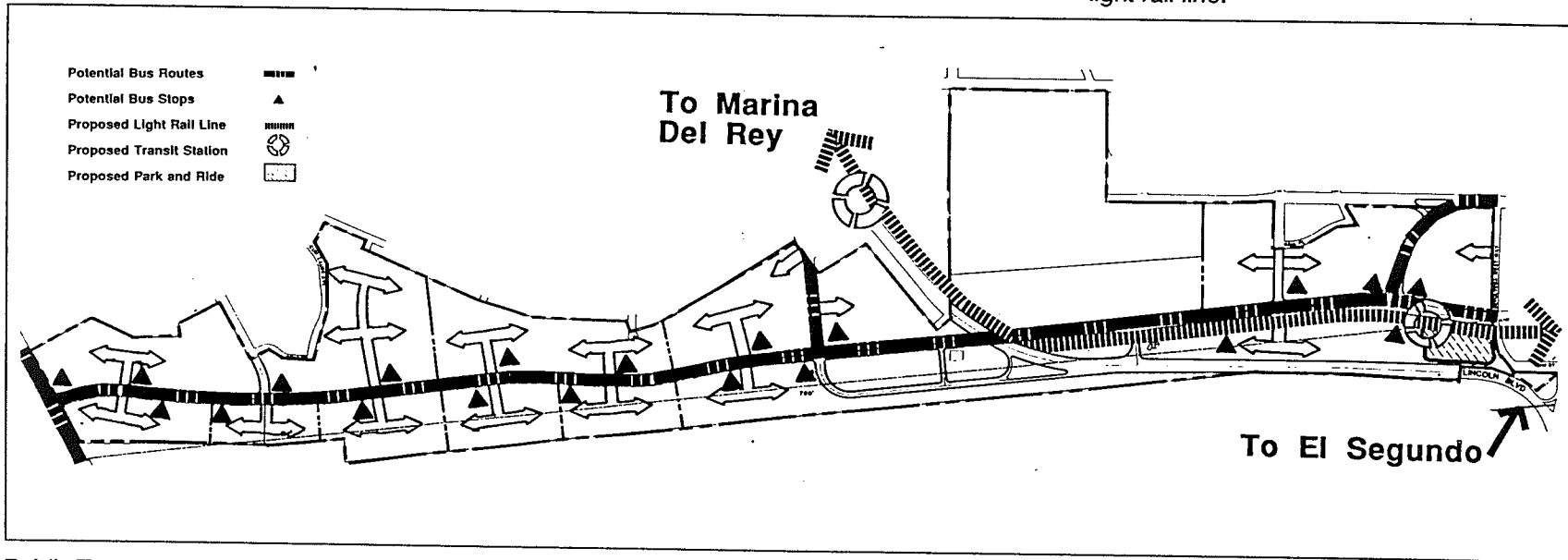
The LACTC should be consulted for the current status of all plans regarding the light rail project before development plans at LAX Northside are finalized.

### Bus Service

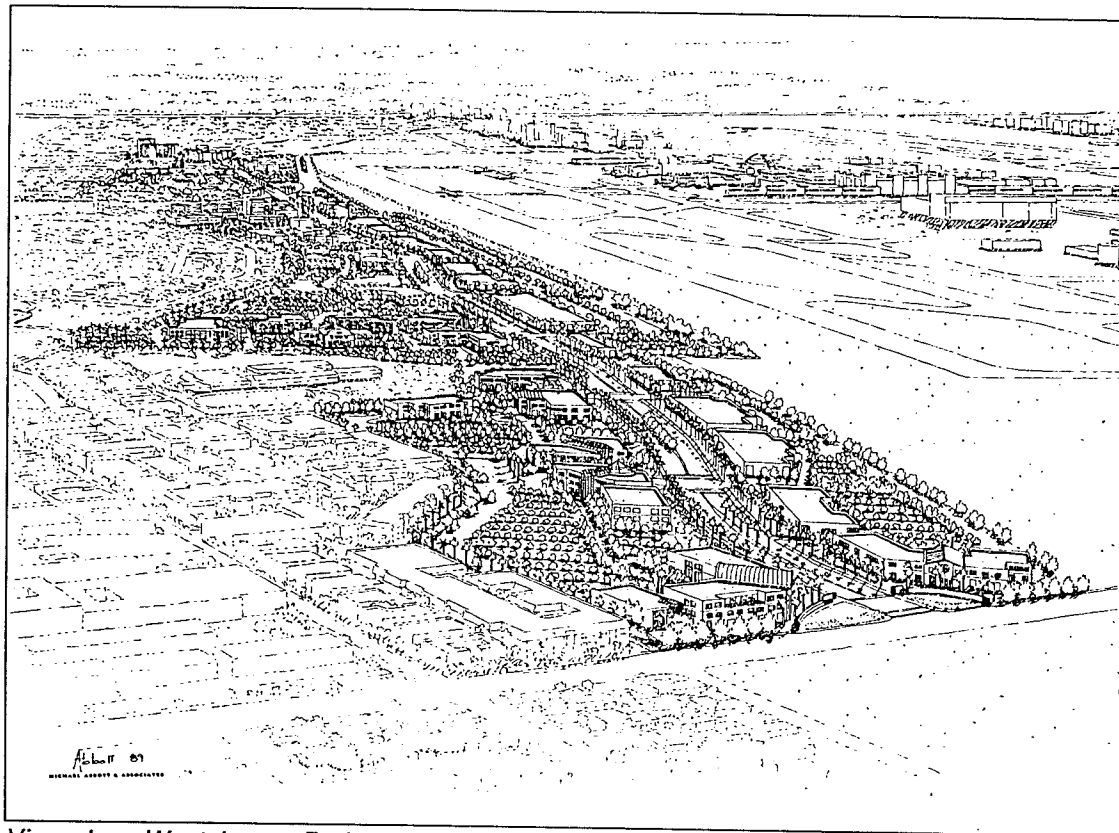
There is currently no bus service to the property, except along Sepulveda Boulevard east of the project, and along Pershing Drive at the western edge of the site. It is probable, however, that after Westchester Parkway has been completed, and development occurs, that RTD will provide bus service along that street. The developer should consult with the RTD, Los Angeles County Transportation Commission, and any other agencies which may operate transit lines within the project, and make provisions for bus stops and other required facilities. If bus service is provided, bus stops along Westchester Parkway should be located near major site entries and should connect with site pedestrian circulation systems.

### Coastal Transportation Corridor Specific Plan

LAX Northside is part of the Coastal Transportation Corridor Specific Plan area adopted by the City of Los Angeles in 1985. The purpose of the plan is to address the present and future needs for transportation within the corridor, which is experiencing serious traffic and transportation problems. Within the project area, Transportation Impact Assessment Fees will be required, based on the number of vehicle trips which a project is projected to generate. Trips are projected based on floor area, and a trip factor contained in the ordinance. The funds generated by this fee will be used to finance identified mitigation measures, which include transportation management, street improvements, such as the Arbor Vitae interchange, and public transportation, such as the light rail line.



*Public Transportation*



View along Westchester Parkway within the Business Park

## 2.5 Urban Design Concept

An urban design concept will mark *LAX Northside* as a special district within the Los Angeles region. The concept takes advantage of the relationships between Los Angeles International Airport, the Westchester Business District and *LAX Northside*, while creating an edge to the existing Westchester residential neighborhoods.

The primary design elements which will unify the project include:

- clearly marked project gateways;
- a consistent street treatment along Westchester Parkway;
- a sequence of spatial experiences along the length of the development;
- large-scale landscape treatment at major intersections;
- a regular pattern of site entries, which incorporate repeating landscape and graphic elements.

Within the overall concept for *LAX Northside*, the two development zones will each have a different character, reflecting the differences of location and land uses.

## LAX Northside—Westchester Center Urban Design Concept

*Westchester Center* offers a special opportunity in urban design for several reasons:

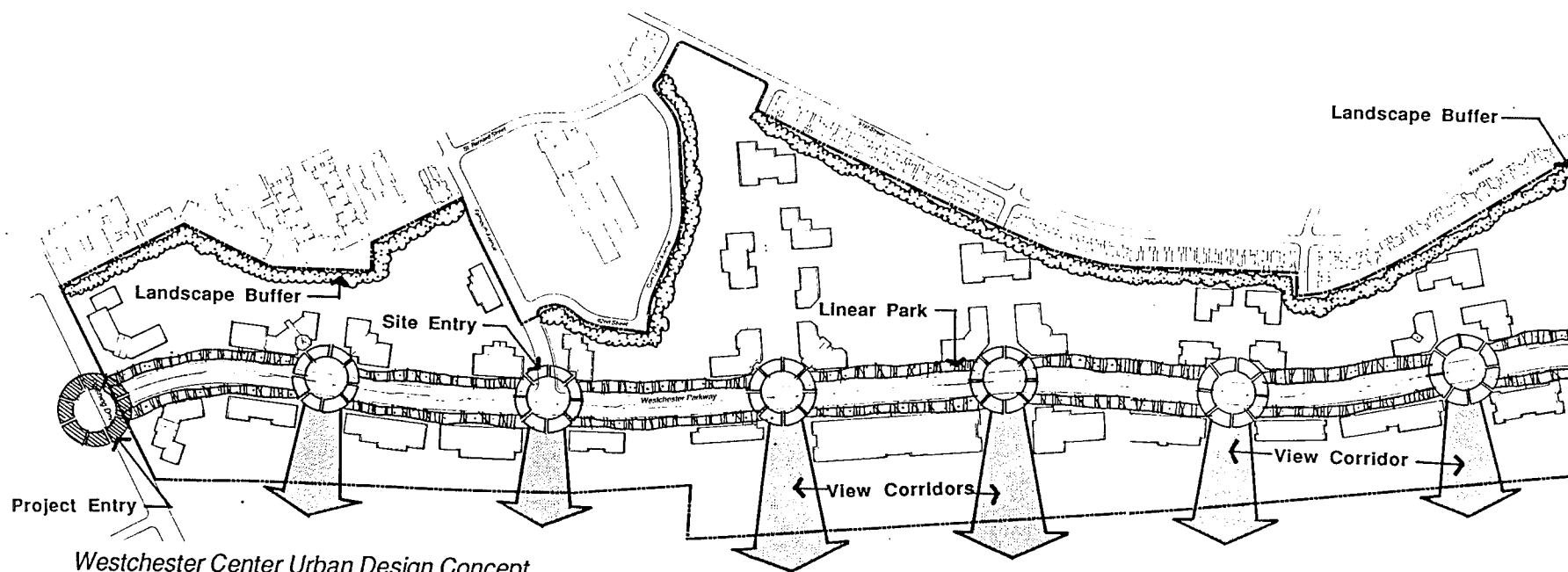
- It will, along with the Westchester Downtown, be the major urban activity center in the area.
- It will be the location of the transit station which is designed to serve not only this project, but the Westchester Business District as well.
- The intersection of Westchester Parkway and La Tijera Boulevard will be a major

traffic carrier with the potential of becoming a focal point of *LAX Northside*.

- The site has high visibility from and views of the airport runways and terminal.
- The western end of the center fronts onto the redeveloped Westchester Golf Course.
- The proximity to the north runway requires mitigation of noise in public areas.

Some of the objectives of the development concept for *LAX Northside-Westchester Center* are:

- To develop *LAX Northside—Westchester Center* as a single master planned project, which integrates multiple uses into a distinctive, enjoyable place.
- To extend the urban pattern of Westchester.
- To present a friendly face toward the existing Westchester Business District.
- To use the golf course in establishing views and vistas.



*Westchester Center Urban Design Concept*

The primary features of the urban design concept for *Westchester Center* are a tightly-scaled pedestrian environment, and an axis which provides visual and pedestrian connections between the various buildings in the center and ties this development with the existing Westchester downtown.

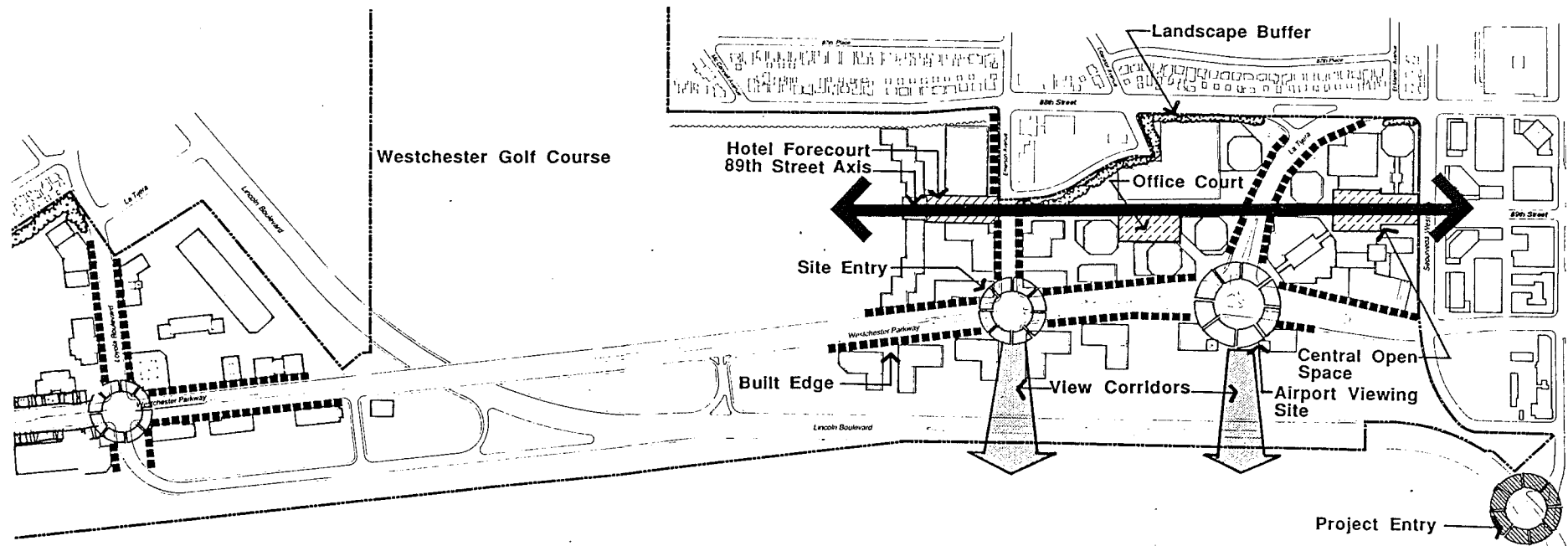
The axis will be formed as an extension of 89th Street. This axis will terminate in the golf course on the west, and be open-ended on the east, providing an opportunity for future con-

nections with the Westchester Business District. This axis will act as an organizing element and may function in places as a driveway, in other places as a strictly pedestrian walkway. It will visually link both sides of La Tijera, although pedestrian crossing will need to take place above grade or at the intersections:

Buildings will define a series of small, landscaped open spaces. These spaces should be along the edge of pedestrian activity and

should provide seating areas and access to the sun with shading provided by trees or umbrellas. These spaces should also be sheltered, when possible, from direct runway noise.

There will be an airport viewing site incorporated into the development south of Westchester Parkway, at the intersection of La Tijera and Westchester Parkway. This site will be designed in conjunction with restaurants or offices on either side which can overview the site and monitor activity there.



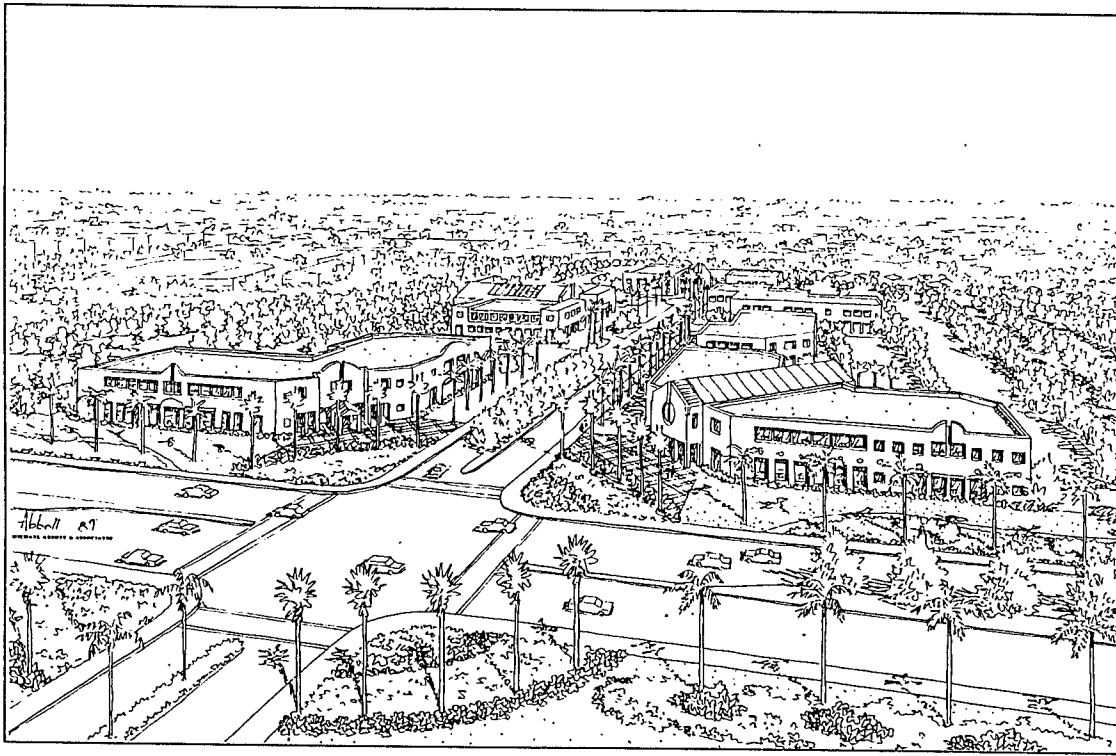
## **LAX Northside—Business Park Urban Design Concept**

The *Business Park* will be a low-rise, suburban office and research park extending from Lincoln Boulevard west to Pershing Drive. The park will be anchored by a commercial center near Lincoln which will achieve a somewhat urban quality and will elsewhere consist of low-scale office and research buildings on the north side of Westchester Parkway and airport-related services along the south side of Westchester Parkway. Along both sides of Westchester Parkway deep building setbacks will be used to increase the visual width of the

roadway and will establish a linear “park” which will link separate developments, and create the primary frontage for the buildings within the *Business Park*.

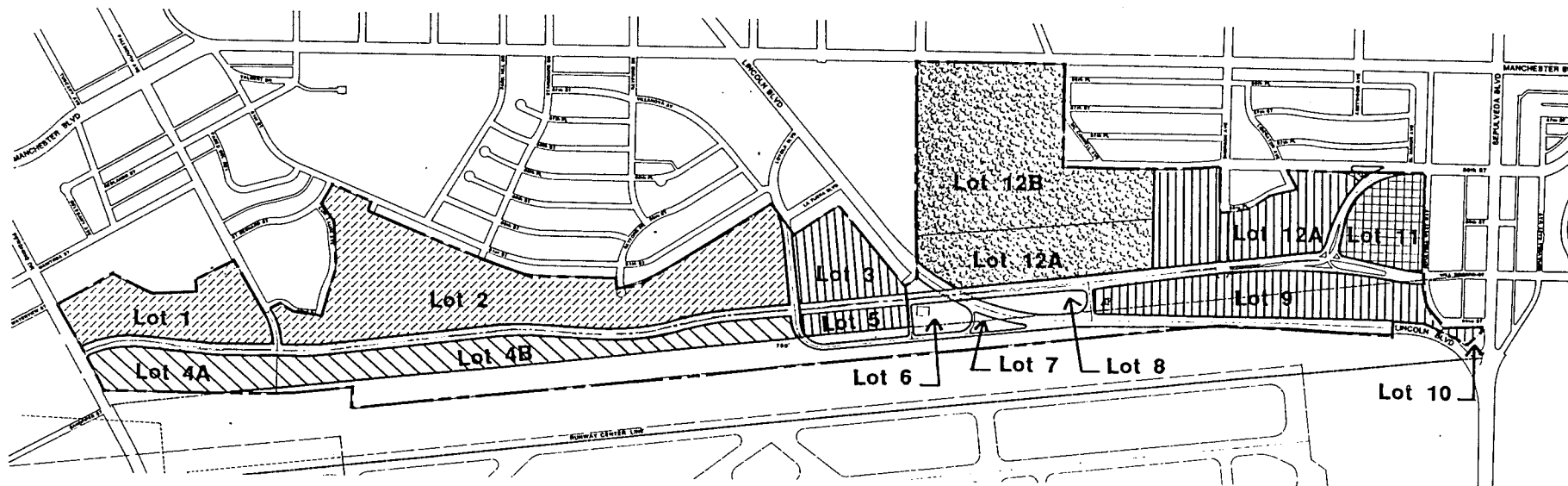
Site access along the road will be limited, and project entry points will become major design features along Westchester Parkway, incorporating graphic and landscape elements. On the south side of the street, wide setbacks at the site entries provide view corridors which allow visual connection between *LAX Northside*

and the airport. Elsewhere along the south side of the Westchester Parkway, setbacks between buildings will be minimized to establish a relatively uniform wall of airport-related service buildings. Along the north side of Westchester Parkway, buildings will maintain a relationship to the street, but will be more diverse, both in siting and design. Wider setbacks between buildings will allow views into the deeper portions of the site.



*Entry Point into Business Park*

# **3.0 DEVELOPMENT PROGRAM**



*Proposed Land Uses*

## Recommended Land Uses

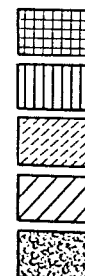
Mixed Use: Hotel / Office / Retail

Commercial: Hotel / Office / Retail

Research Park

Airport Related Uses

Golf Course





### 3.1 Land Use

#### Background

The zoning for the site allows a range of commercial and light industrial uses on most of the parcels.

Market studies identified strong potential for hotel, office and light industrial uses, especially those directly related to the airport. The retail market is weak, except that which would be generated by on-site development. The airport has committed itself not to directly compete with existing Westchester retail.

#### Objectives

- To create a mix of land uses which responds to the needs of the market, while being compatible with adjacent neighborhoods.
- To develop the site to the maximum potential allowed by the zoning.
- To concentrate hotel and office uses in an area where they have access to public transportation, can utilize common services, and help provide a market base for the existing Westchester retail district.

#### General Guidelines

1. Land uses should be distributed so that hotel and office uses are concentrated in the east part of the site (*Westchester Center*), with industrial and research park uses to the west (*BusinessPark*).
2. Retail uses will consist of service and food establishments which are designed to serve the needs of the business and hotel interests. Retail uses should complement rather than compete with retail uses within the existing Westchester Business District. Large department stores or other retail uses drawing on a regional market will not be permitted.
3. Retail and restaurant uses should be clustered in two small centers: In Lot 3 (at the corner of Loyola Boulevard and Westchester Parkway), and in Lots 9, 11 and 12 (at the intersection of La Tijera and Westchester Parkway). These two locations will each provide an identifying focal point for the two zones of the project.

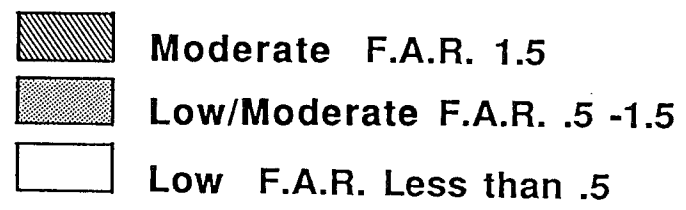
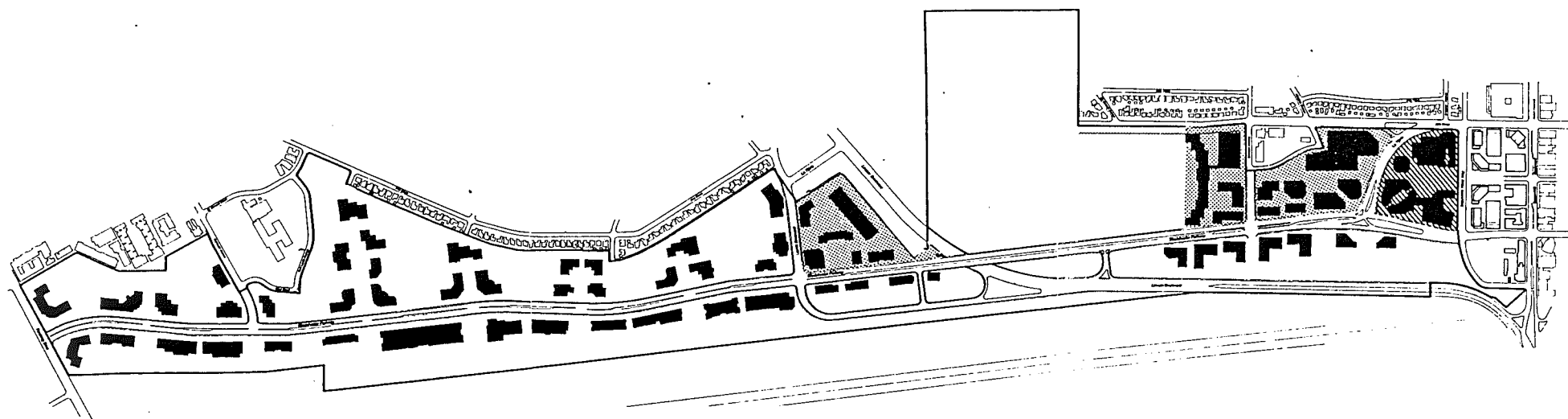
#### Research Park Land Uses

Lot 1	Research Park	21.3 Ac*
Lot 2	Research Park	69.5 Ac
Lot 3	Office, Hotel	
	Retail/Restaurant	12.5 Ac
Lot 4	Airport-Related Uses	57.0 Ac
Lot 5	Office	6.0 Ac

#### Westchester Center Land Uses 166.3

Lot 9	Office	
	Restaurant	
	Airport View Site	3.0 Ac*
Lot 11	Mixed Use:	
	Office, Hotel, Retail	11.4 Ac
Lot 12A	Hotel Restaurant	
	Office	24.0 Ac
Lot 12B	Golf Course	78.0 Ac

\* All acreages approximate 116.4 Ac



*Building Density*

## 3.2 Density

### Background

The zoning conditions for the site set a project-wide limit on density based on traffic generation. Project generated traffic will be limited to 7,000 project-related outbound daily trips in the p.m. peak hour, and 6340 project-related inbound daily trips in the a.m. peak hour. Based on traffic generation estimates used in the EIR, this limits development to around 4,500,000 s.f., or an average floor area ratio of 0.43 for the development parcels. Effective Transportation Systems Management measures will be required in order to limit traffic generation to the estimated volumes.

Actual trip generation will be monitored after 3,500,000 s.f. of development has occurred. Measured traffic generation will be used as the determination of how much development beyond that level will be permitted. If effective measures are implemented to limit peak hour traffic generation, then more than 4,500,000 may be permitted. If traffic reduction measures are ineffective, then it is possible that development will be limited to some point between 3,500,000 and 4,500,000 s.f.

The proposed light rail station adjacent to Lot 11 offers an opportunity for a greater density of uses within walking distance of the station.

LAX Northside is located in Height District 1, and no single lot may exceed a floor area ratio of 1.5:1, according to the City of Los Angeles Zoning Code.

Other factors which affect density are the zoning conditions and FAA setbacks and height limits. See Sections 2.2, 2.3, and 5.2.

### Objectives

- To create a critical mass which will give the *Westchester Center* an identity as a center, and provides the volume of activity to support service facilities such as retail and restaurants.
- To locate the highest density uses adjacent to existing and planned public transportation facilities
- To maintain lower densities in the portions of the project adjacent to residential neighborhoods.

### General Guidelines

1. Within the overall density cap placed on the site by the "Q" conditions, *Westchester Centers* should be built at relatively urban densities, while *the Business Park* should be at a much lower density.
2. The following program shown in Table 1 is a recommendation of uses by floor area and by lot. Some variation is possible based on market conditions and the needs of individual users. Any deviation from the program must not increase the total projected traffic generation. The traffic generation factors used in the EIR are shown in Table 2.

### Westchester Center Guidelines

1. Lot 11 should contain a mix of office, hotel and retail uses. No single use should occupy more than 60% of the total floor area of the block. There should be a maximum of 50,000 s.f. of retail uses.
2. Retail uses should be concentrated in an enclosed court which offers some protection from airport noise, but maintains street visibility and has adjacency to the hotel, offices and transportation.

### TRIP GENERATION FACTORS

LAND USE CATEGORY	TRAFFIC VOLUME PER 1,000 SQ. FT. OF FLOOR AREA				
	AM Peak Hour		PM Peak Hour		Daily 2-Way Volume
	IN	OUT	IN	OUT	
1. Low-Rise Office	1.90	0.10	0.20	1.90	12.00
2. Airport-Related	1.30	0.30	0.70	1.60	10.30
3. Research Park	1.20	0.10	0.30	1.20	8.20
4. Hotel	0.80	0.80	0.70	0.70	20.00
5. Sit-Down Restaurant	0.80	0.50	7.30	4.70	105.00
6. Specialty Retail	0.30	0.30	2.50	2.50	50.00
7. Moderate-Rise Office	2.20	0.10	0.30	2.30	14.00

Source: Final EIR, LAX North Side Development Project

## LAX Northside Recommended Program

Program Uses In Thousands of Square Feet

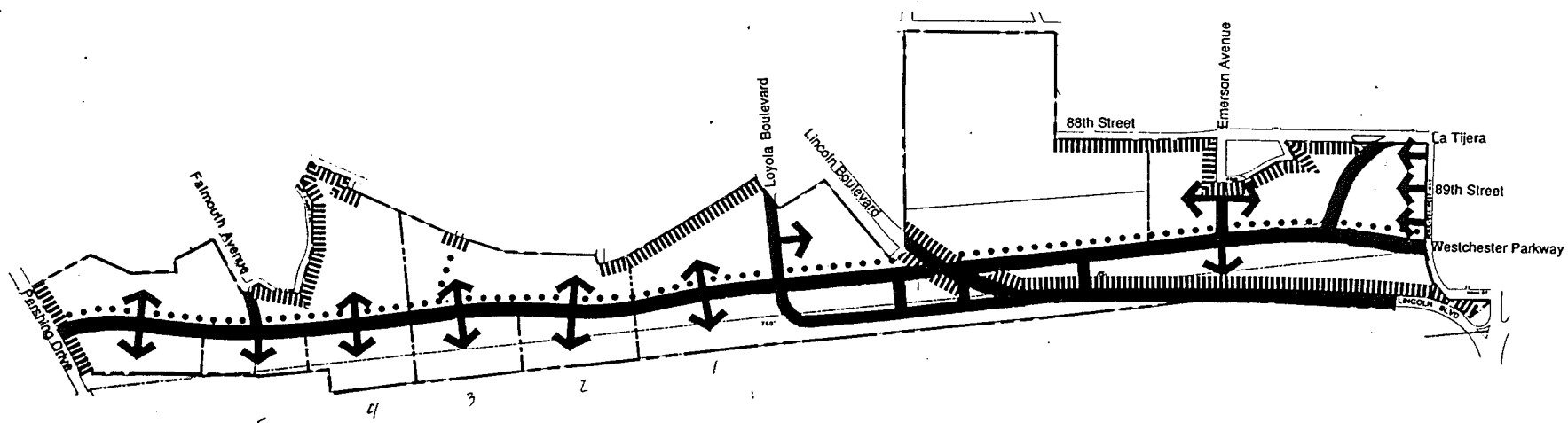
Lot No.	Site Area	Retail	Restaurant	Hotel	Mid-rise Office	Low-Rise Office	Research Park	Airport Support	Total	F.A.R.	Parking <sup>1</sup>	PM Peak <sup>2</sup> Hour
1	21.3 Ac						270		270	.3	810	320
2	69.5 Ac						900		900	.3	2700	1080
3	12.5 Ac	10	20	220 (400 Rooms)		140			390	.75	1290	540
4	57 Ac <sup>3</sup>							750	750	.3	1875	1200
5	6.0 Ac					80			80	.3	270	150
6	3.1 Ac								0	0	0	0
7	1.0 Ac								0	0	0	0
<b>Subtotal Business Park</b>	<b>170.4 Ac</b>	<b>10</b>	<b>20</b>	<b>220</b>		<b>220</b>	<b>1170</b>	<b>750</b>	<b>2390</b>	<b>.32</b>	<b>6945</b>	<b>3290</b>
8	3.0 Ac								0	0	0	0
9	28.2 Ac		20			400			420	.3	1640 <sup>4</sup>	855
10	0.7 Ac								0	0	0	0
11	11.4 Ac	50	20	390 (600 Rooms)	280				740	1.5	2145	1130
12	24.0 Ac <sup>5</sup>		10	260 (400 Rooms)	580	100			950	.84	2870	1785
<b>Subtotal Westchester Center</b>	<b>69.3 Ac</b>	<b>50</b>	<b>50</b>	<b>650</b>	<b>860</b>	<b>500</b>			<b>2110</b>	<b>.7</b>	<b>6655</b>	<b>3825</b>
<b>Total</b>	<b>239.7 Ac</b>	<b>60</b>	<b>70</b>	<b>870</b>	<b>860</b>	<b>720</b>	<b>1170</b>	<b>750</b>	<b>4500</b>	<b>.43</b>	<b>13600</b>	<b>7030</b>

### Notes

1. Minimum parking based on zoning requirements. Changes in City of Los Angeles parking requirements are under consideration, and may increase requirement.
2. PM Peak Hour traffic determined by trip generation factors used in EIR.
3. Total acreage of Lot 4 is approximately 64 acres, of which 7 are occupied by the drainage channel.
4. Does not include 250-500 car Park and Ride.
5. Total Acreage of Lot 12 is approximately 102 acres, of which 78 are assumed to be allocated for the golf course.

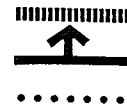


# **4.0 SITE ACCESS GUIDELINES**



*Vehicular Access*

Prohibited Access  
 Major Access Point  
 Bikeway





## 4.1 Vehicle Access, Driveways, and Parking

### Background

A number of factors determine and limit vehicular access to the development parcels. The zoning conditions prohibit direct site access from Lincoln Boulevard, Pershing Drive, and all the local streets along the north edge of the site, except for Falmouth Avenue. Emerson Avenue will be blocked at the north property line to prevent through traffic.

La Tijera Boulevard curves as it passes through the site, making left turn access to adjoining sites undesirable.

Major access drives, allowing left turns, along Westchester Parkway will be limited to enhance traffic flow and to reduce the disruption of the landscaping and medians.

Only Sepulveda Westway, Emerson Avenue from the south, Loyola Boulevard, and Falmouth Avenue, allow free access, and are of limited value to most development sites.

Major access drives along Westchester Parkway will provide the only access for most of the development sites within the research park. Each access will potentially serve several buildings, depending on the size of buildings and the final development plan. In addition to these major entries there may be additional entries allowing right turn only access to development sites.

### Bikeways

Westchester Parkway is designated as a planned bicycle route within the City of Los Angeles Bicycle Plan. As part of the design of the road, a striped bike path within the roadway will be constructed. In addition, the "Q" conditions require that there be a bike path between Stanmoor Drive and Westchester Parkway, to the satisfaction of the Department of Transportation and the City Council Office.

### Objectives

- To coordinate entrance drives with each other to create a simple, easily recognizable pattern of major and minor entries.
- To locate parking so that it is convenient and easy to find, but does not dominate the frontage along Westchester Parkway. Within the *Business Park* most of the parking will be located in large surface lots; within *Westchester Center* parking will be located in above-or-below grade structures, in small short-term lots north of Westchester Parkway and in surface lots within a no-building zone south of Westchester Parkway.

### General Guidelines

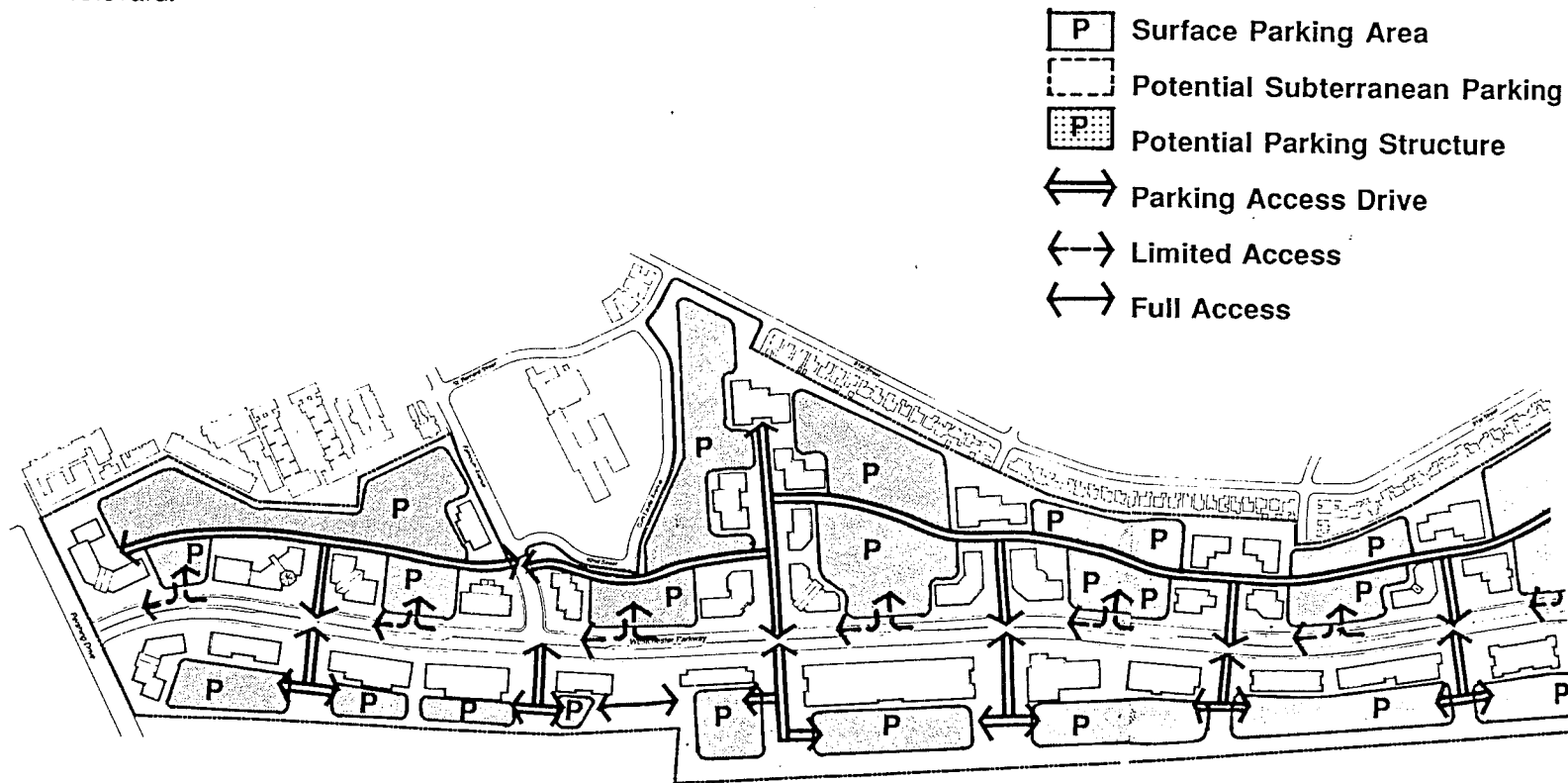
1. Major access points will be coordinated with median openings which will occur at intervals of 850 to 1,000 feet. Between the major access points or street intersections, there may be up to two minor access points designated for right turns only.

These minor access points should be no closer than 250 feet to a major access point or street intersection. All access drives will need to be approved by the City of Los Angeles Department of Transportation.

2. Driveways and parking areas should be designed to accommodate efficient vehicle stacking during peak periods, based on a site specific traffic analysis, to be approved by the City of Los Angeles Department of Transportation.
3. Large parking areas should be located away from Westchester Parkway, i.e., on the north side of the buildings on the north side of the street, and on the south side of the buildings on the south side of the street. Parking will not be permitted between buildings fronting on Westchester Parkway and the street.
4. All parking facilities should give priority to the convenient location of adequate van, carpool and bicycle parking.
5. Bicycle parking areas with racks having locking capabilities shall be provided. Retail or restaurant uses should provide bicycle parking near entrances in highly visible areas. Office or research buildings should provide secured bicycle parking near employee entrances.

### Westchester Center Guidelines

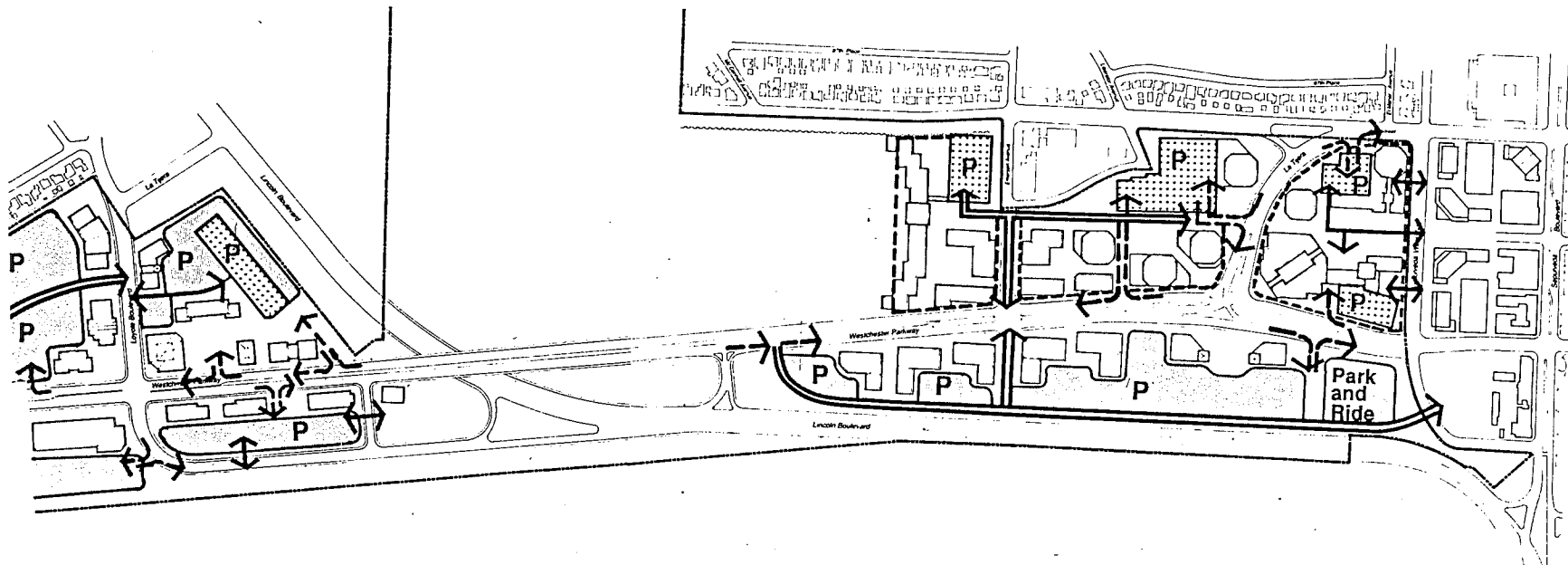
1. Parking will be provided primarily in above and below-grade structures, north of Westchester Parkway. Parking structures should be located so that they do not create the primary frontage on Westchester Parkway, Sepulveda Westway, or La Tijera Boulevard.
2. Access to parking structures will be primarily from Sepulveda Westway and Emerson Avenue, with minor access from Westchester Parkway and La Tijera.
3. Access to parking should be clearly marked but should not dominate the street frontages. Building entrances and pedestrian zones should create the primary focus from the streets.



*Vehicular Access Concept*

### Business Park Guidelines

1. Full access drives, providing left and right turns into parcels, will be limited to not more than four drives between Loyola Boulevard and Falmouth Avenue, and one drive between Falmouth and Pershing Drive. No full access drives will be permitted between Lincoln and Loyola Boulevards.
2. An additional minor access point providing right turn only access may be provided at mid-blocks and between full access points.
3. Access drives parallel to Westchester Parkway should provide connections between full and minor access points, interior parking areas, and any buildings located between major access drives.
4. Direct vehicular access between airport related users and the airport road system may be permitted under some circumstances. Airport security and safety requirements must be met. Bridging of the existing drainage channel may be necessary and will require approval of the Department of Airports.



## 4.2 Pedestrian Circulation

### Background

Westchester Parkway and other dedicated streets within the project will be built with sidewalks on both sides to City of Los Angeles Department of Transportation standards.

An adequate pedestrian circulation system, linking buildings, streets, parking areas, and public transit stops is essential in creating an environment which encourages employees to give up their private automobiles and use public transportation, vanpools, and carpools.

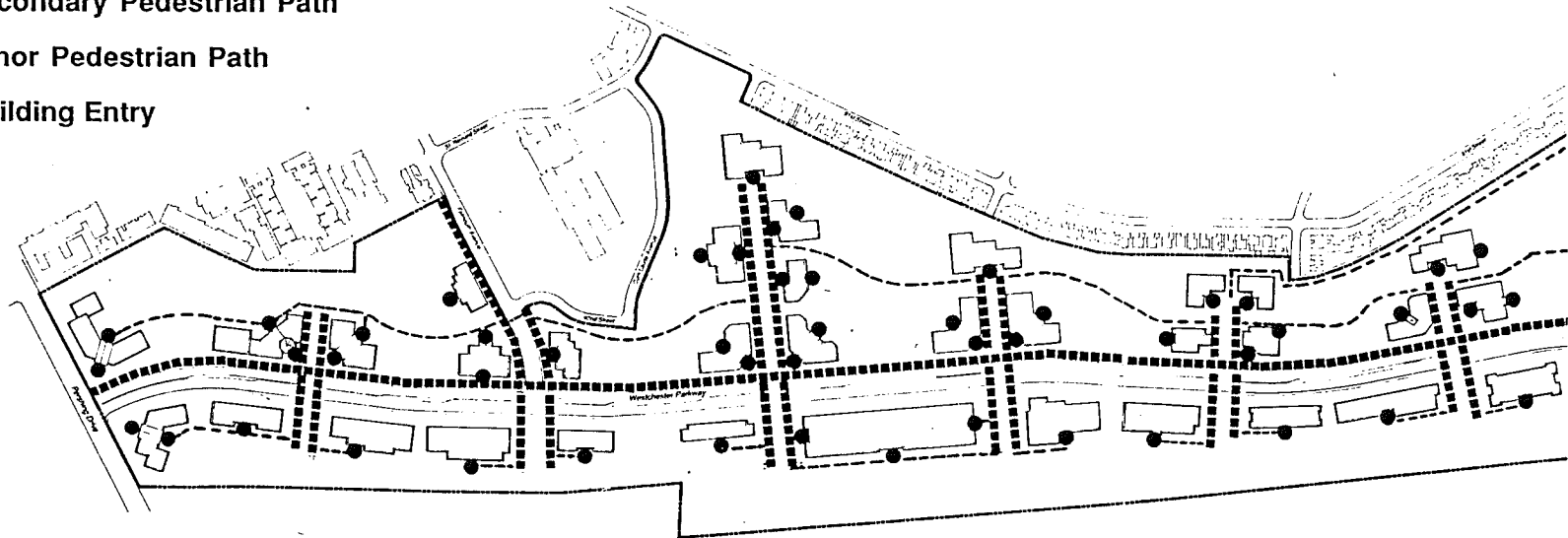
### Objectives

- To tie all other pedestrian systems into the sidewalk network.
- To provide convenient and direct pedestrian connections between work places, public transportation, and services such as restaurants.

### General Guidelines

1. The pedestrian circulation system should provide direct, paved connections between parking areas and the buildings they serve.
2. Retail and publicly oriented service should be concentrated along primary pedestrian paths.
3. Pedestrian amenities and building openings should be used to reinforce primary and secondary paths.
4. All pedestrian areas and destinations should be handicap-accessible.

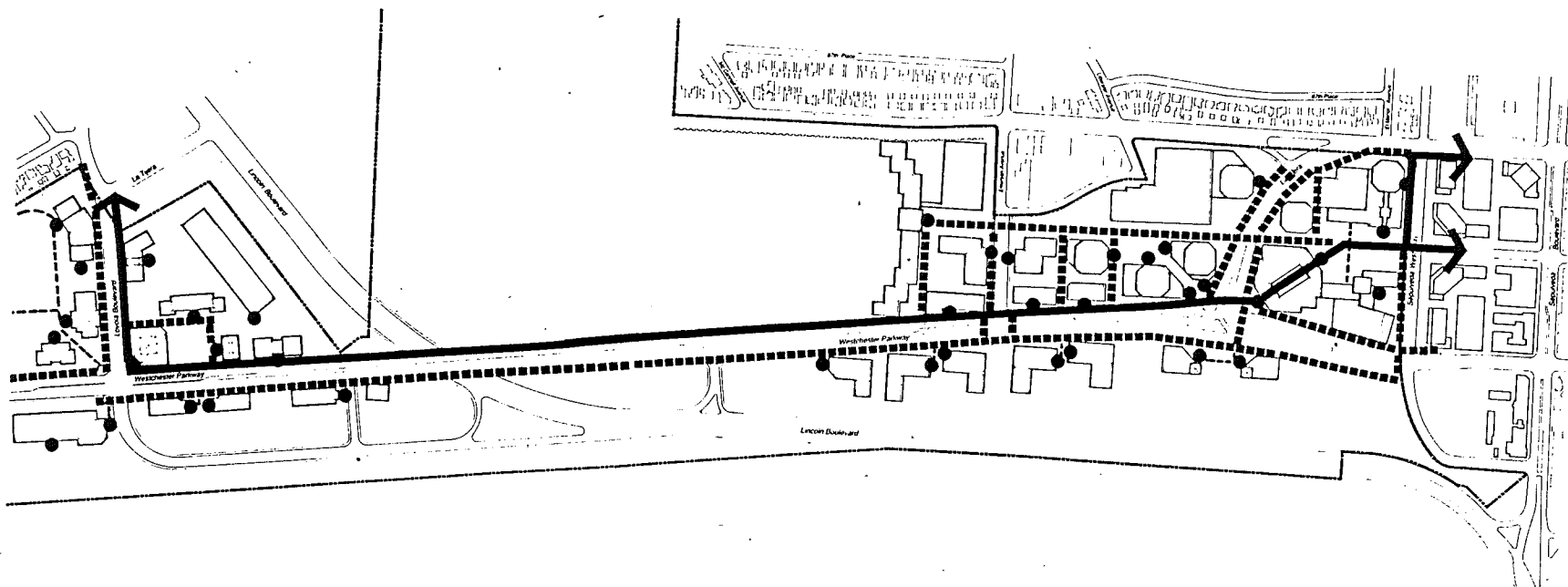
- Primary Pedestrian Path
- - - Secondary Pedestrian Path
- - - Minor Pedestrian Path
- Building Entry



*Westchester Center Pedestrian Circulation Concept*

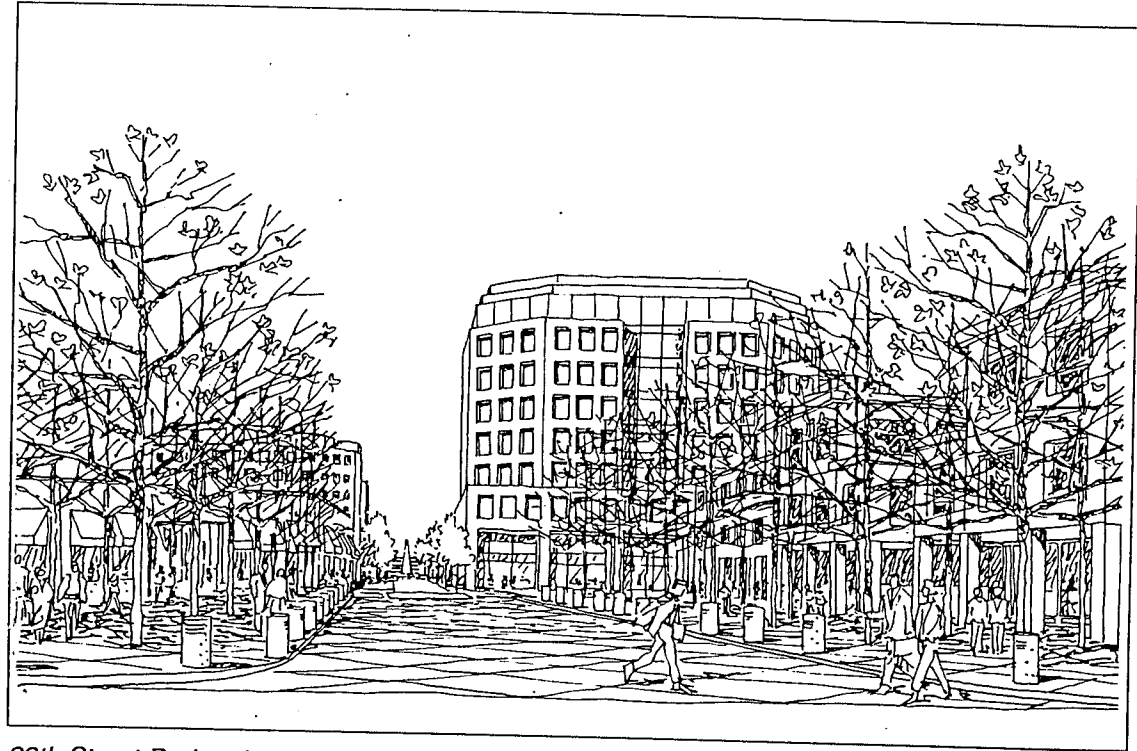
### Westchester Center Guidelines

1. The sidewalk system should provide the basic framework of the pedestrian network. However, there should be additional paths which run through the center of the sites and provide more direct connections between buildings. These paths should be clearly designated by special landscaping and paving materials.
2. There should be pedestrian paths through Lot 11 connecting the transit station with the Westchester Business District to the north and east. These paths should be direct, but may cross through the public spaces of buildings, such as the retail court. The paths should be broken into a series of smaller segments, and should not pass through service areas or alley-like spaces.
3. Every major building should have a main entrance oriented toward the street-side of the building, connecting with the sidewalk. There may be additional entries oriented toward interior courts and pedestrian paths.



### Business Park Guidelines

1. North of Westchester Parkway there should be direct pedestrian connections between the sidewalk and main building entrances.
2. South of Westchester Parkway, building entrances may face away from the street.
3. A continuous pedestrian path north of and generally parallel to Westchester Parkway should be provided which connects buildings and site amenities such as outdoor seating or lunch areas. This path may be combined with a jogging trail.
4. Shelters offering protection from the sun and rain as well as providing seating shall be provided at all bus stops as part of the pedestrian system and landscaping of adjacent development.



*89th Street Pedestrian Axis*

# **5.0 ARCHITECTURAL GUIDELINES**

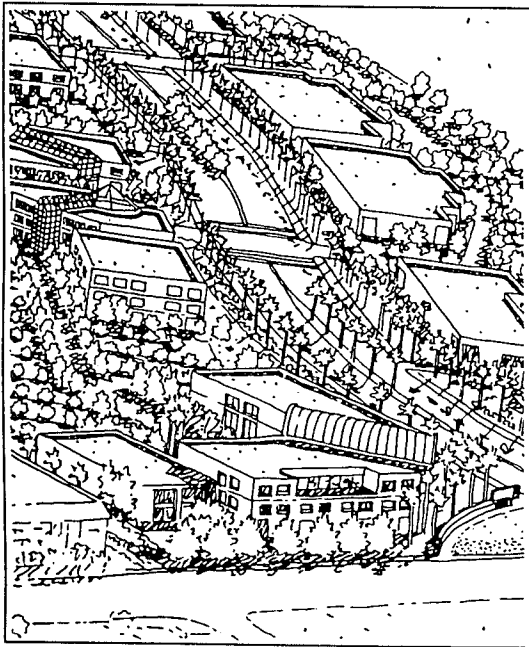




## 5.1 Building Setbacks, Streetwall and Orientation

### Background

The purpose of the architectural guidelines is to establish a unifying design theme within which individual variations may occur. The overall effect of a group of buildings united by common frontage, related massing, colors and materials is a much more cohesive effect than a series of special object buildings typically found in office parks and commercial developments.



Streetwalls

As part of the zoning conditions a number of setbacks from the northern property line are required. These setbacks establish the minimum buffer required between new development and existing neighborhoods. (See Section 2.2)

The FAA requires that all structures be setback a minimum of 750' from the centerline of the north runway. (See Section 2.3)

It is the intention that structures along the parkway will form a unified streetwall, broken only for access roads and view corridors. This streetwall will be the most important architectural element in unifying separate developments and relating buildings to the streets they face. Streetwalls are the most important tool for keeping any one building from dominating others. The walls provide continuity to the street and at the same time allow decorative touches and changes which guarantee individual expression and distinction to each building.

In *Westchester Center*, the wall will be quite continuous, with breaks minimized. In the *Business Park*, the wall will be much less continuous, but will be used to define the space of the parkway as a linear park. Buildings will front onto Westchester Parkway, which will unify and give identity to the entire development.

### Objectives

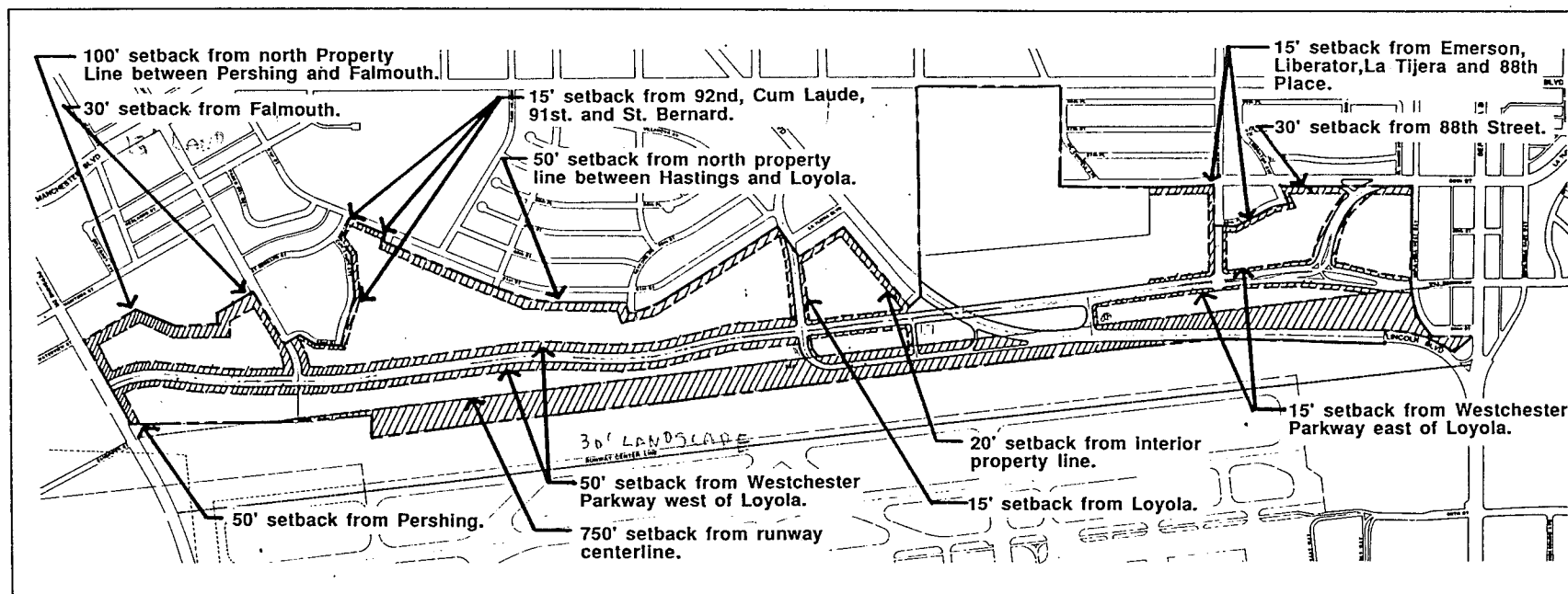
- To allow sufficient space for landscaping and active use of the space along Westchester Parkway.
- To allow views into deeper portions of the site at key places.
- To allow views between buildings toward the airport.
- To provide for buffers between buildings and adjacent streets and development.
- To use buildings to define a consistent front along Westchester Parkway and give spatial definition to the street.

### General Guidelines

1. All buildings shall be set back a minimum of 750 feet from the centerline of the north runway, as required by the FAA.
2. All buildings shall conform to setback requirements established by the "Q" conditions.

### Westchester Center Guidelines

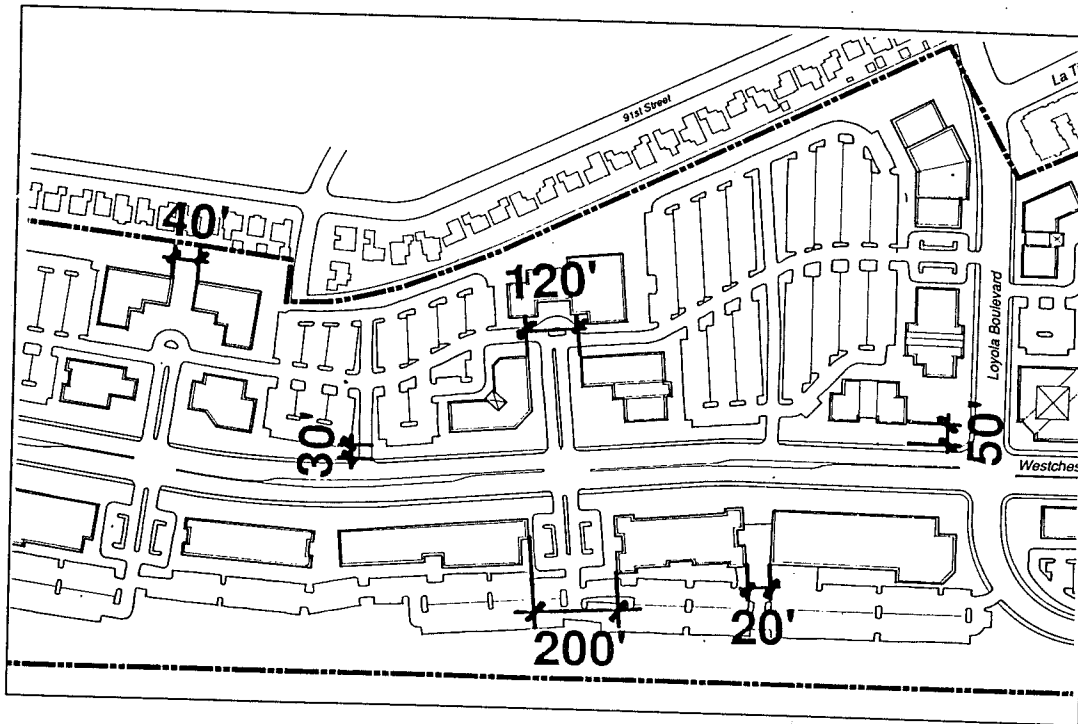
1. Buildings will be set back 15 feet from the property line along Westchester Parkway, La Tijera Boulevard and Emerson Avenue. Buildings need not follow this line, but they should come out to it unless prevented from doing so by the proposed light rail alignment.
2. At the intersection of La Tijera Boulevard and Westchester Parkway, buildings on the south side of Westchester Parkway should be set back a minimum of 100 feet from an extension of the centerline of La Tijera Boulevard. This area will be used as a special site for recreational viewing of the airport operations.
3. A 10 foot setback will be required along Sepulveda Westway.
4. FAA requirements will establish setbacks from Lincoln Boulevard.
5. In the areas adjoining the Westchester Business District, major building forms should be oriented so that they are orthogonal to (lined up with) the existing street grid, rather than following the irregular alignment of Westchester Parkway and La Tijera.
6. The relationship of *Westchester Center* buildings to the street is intended to be consistent. Buildings should be built to the setback lines described above. This pattern should not be broken except in the case of carefully selected open spaces.



Building Setback Summary

### Business Park Guidelines

1. Buildings should be set back a minimum of 50 feet from Pershing Drive and Westchester Parkway west of Loyola Boulevard. Within this setback, a minimum of 30 feet adjoining the right of way should be landscaped; the remaining 20 feet may be used for parking or service areas.
2. East of Loyola Boulevard the buildings should be set back 15 feet from the property line of Westchester Parkway.
3. Buildings shall be set back a minimum of 30 feet from the final right-of-way of Falmouth Avenue. Of this, a minimum of 15 feet adjoining the right-of-way should be landscaped.
4. It is desirable that all buildings facing Westchester Parkway should bring their front walls as close as possible to the setback line, and should have their long walls parallel to the north runway of LAX.



*Business Park Building Separations*

5. Buildings may vary from the setback line as long as the spatial definition established by the entire surface of the building along the setback line is maintained.
6. Buildings and parking should be set back a minimum of 15 feet from Loyola Boulevard.
7. Buildings south of Westchester Parkway should be set back a minimum of 100 feet from the extension of the centerline of Falmouth Avenue, and from the centerline of each major entry drive. Buildings north of Westchester Parkway should be set back a minimum of 60 feet from the centerline of each major entry drive into the site, perpendicular to Westchester Parkway.
8. Buildings should be set back a minimum of 20 feet from all interior property lines, except on the south side of Westchester Parkway in *LAX Northside* and west, where buildings may have a minimum of 10' site setback from interior property lines.
9. Buildings should in general be oriented at right angles to the main entrance drives, which will be perpendicular to Westchester Parkway.

## 5.2 Building Height

### Background

Two significant factors, other than these guidelines, specifically control building heights within *LAX Northside*.

- The "Q" conditions restrict building heights in Lots 1, 2 and 4.

In "Parcels 1 and 4A (Parcel 4 west of Falmouth Avenue) no structure located within 200 feet of the north airport property line between Falmouth Avenue and Pershing Drive shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. No structure located within Areas 1 and 4A south thereof shall exceed four stories including parking levels or 55 feet in height measured from the finished grade of the lot".

In "Parcel 2, no structure located within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot". ( from "Q" Condition No. 3)

Mechanical and other accessory facilities located on the roof are exempted from the height restriction but must not be visible from nearby residential properties to the north.

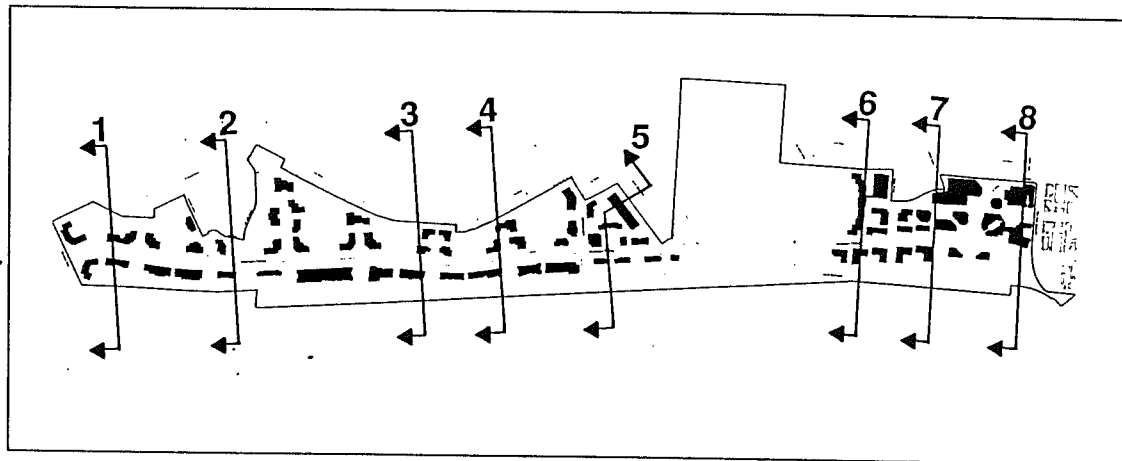
- The Federal Aviation Administration (FAA) regulates the heights of structures in the vicinity of the airport. See Section 2.3 for a description of these restrictions.

The design concept is that the buildings in the *Business Park*, except for the area between Loyola and Lincoln Boulevards, will be uniformly low and evenly distributed along Westchester Parkway. The buildings will help to define the landscape band along the parkway, and be closely related to the landscape. The buildings will, for the most part, be similar in height to the trees along Westchester Parkway, and at their maximum height, maintain a close relationship to the landscape. East of Loyola Boulevard, the proximity to Lincoln Boulevard and the greater mix and intensity of

uses, make greater building heights desirable. A varied and sculptured skyline, seen from Lincoln Boulevard and the airport runways, would make a significant addition to *LAX Northside* image.

### Objectives

- To establish relatively uniform building heights west of Lincoln Boulevard.
- To allow maximum building heights within existing constraints, east of Loyola Boulevard.
- To establish a relatively uniform streetwall height in *Westchester Center*, but encourage an articulated skyline behind the streetwall.



Key Map

### General Guidelines

1. All structures shall conform with the requirements of the "Q" conditions.
2. All improvements shall conform to FAA regulations.
3. As required by City of Los Angeles Zoning Code, all buildings should not exceed the

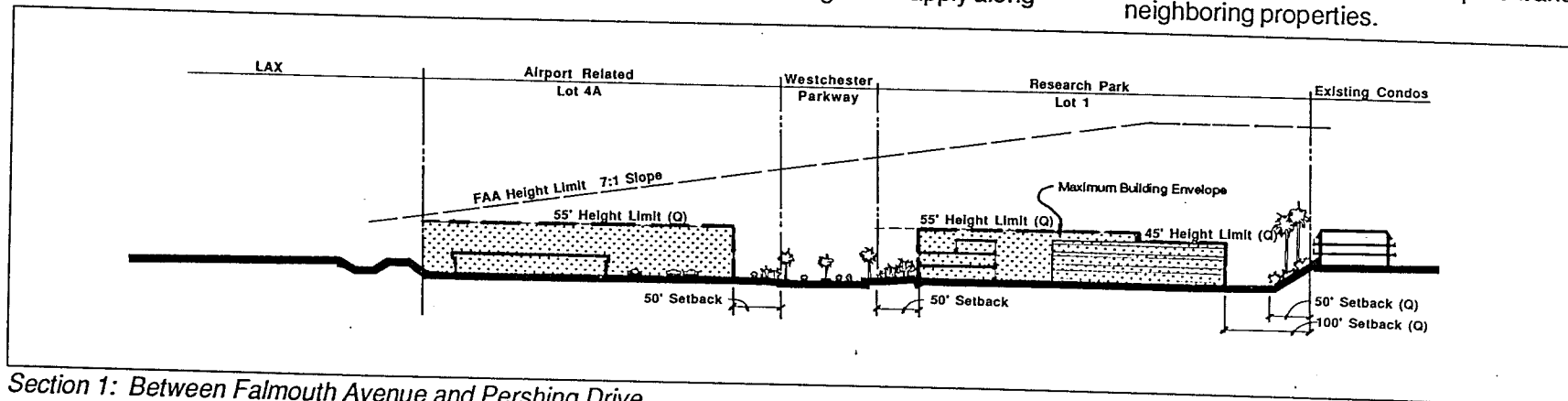
following transitional heights, when located within the distances specified from a single family residential neighborhood:

Distance	Height
0-49 feet	25 feet
50-99 feet	33 feet
100-199 feet	61 feet

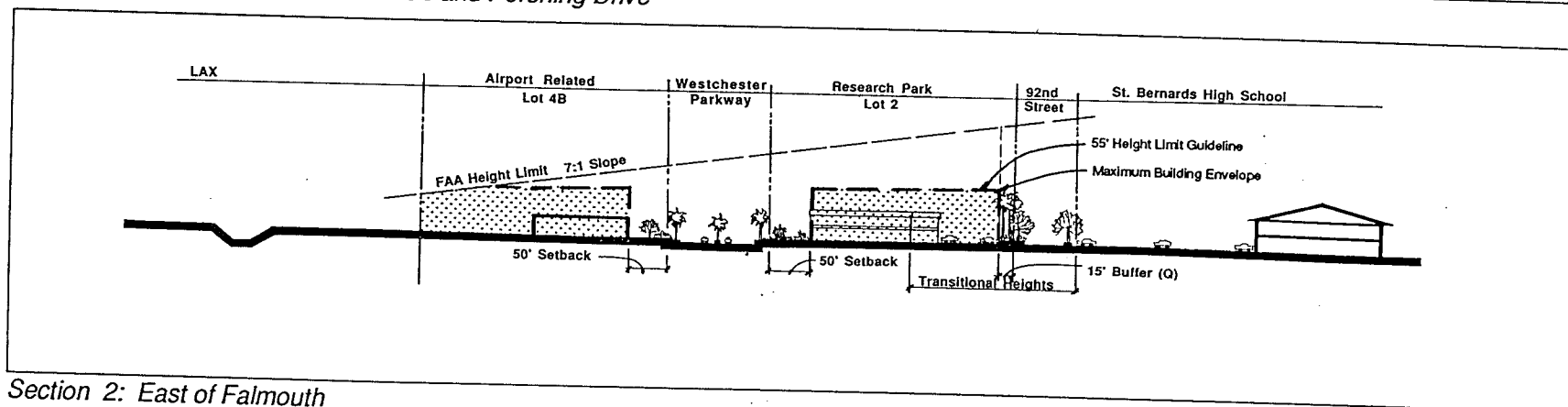
These transitional heights will apply along

88th Street between Sepulveda Westway and the golf course, and along the north boundary between Loyola Boulevard and Falmouth Avenue.

4. The cross-sections which follow should be used as a guideline in determining maximum building heights, and the desired relationship between site development and neighboring properties.



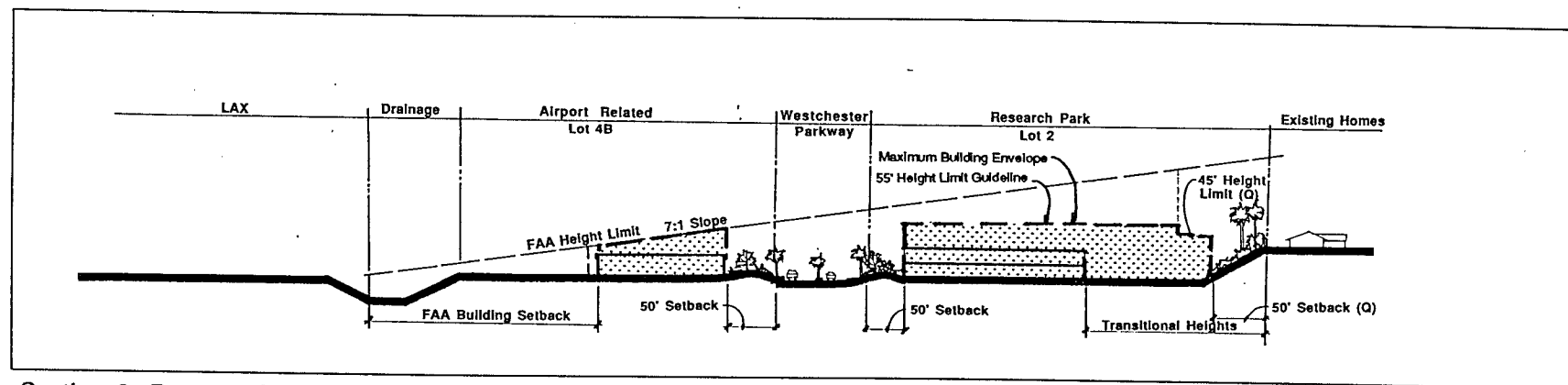
Section 1: Between Falmouth Avenue and Pershing Drive



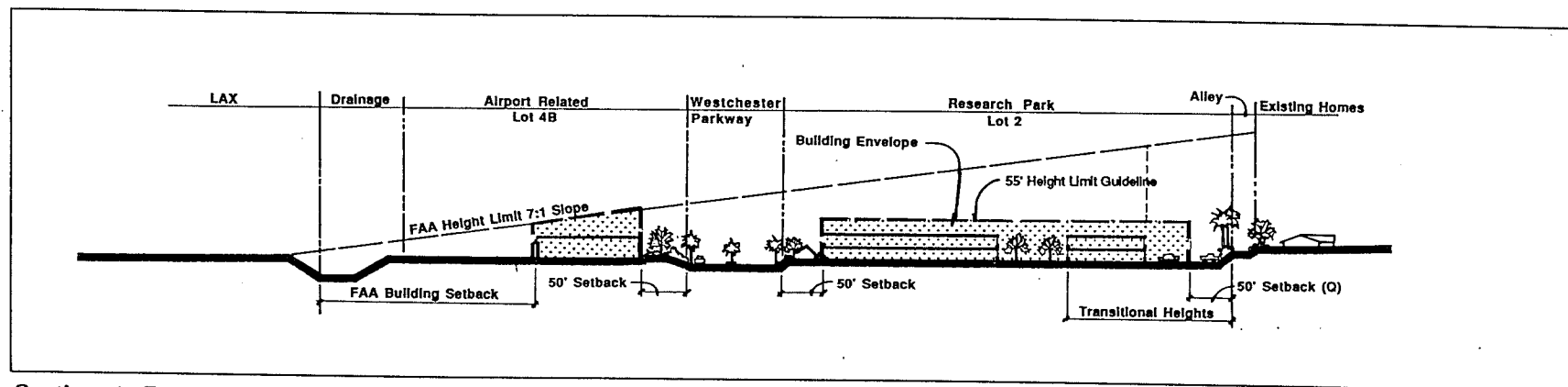
Section 2: East of Falmouth

### Business Park Guidelines

1. Between Lincoln and Loyola Boulevards, structures may be built to the maximum heights achievable under FAA requirements.
2. West of Loyola Boulevard, no building should exceed three stories or 55 feet in height.



*Section 3: Between Stanmoore Drive and Rayford Drive*

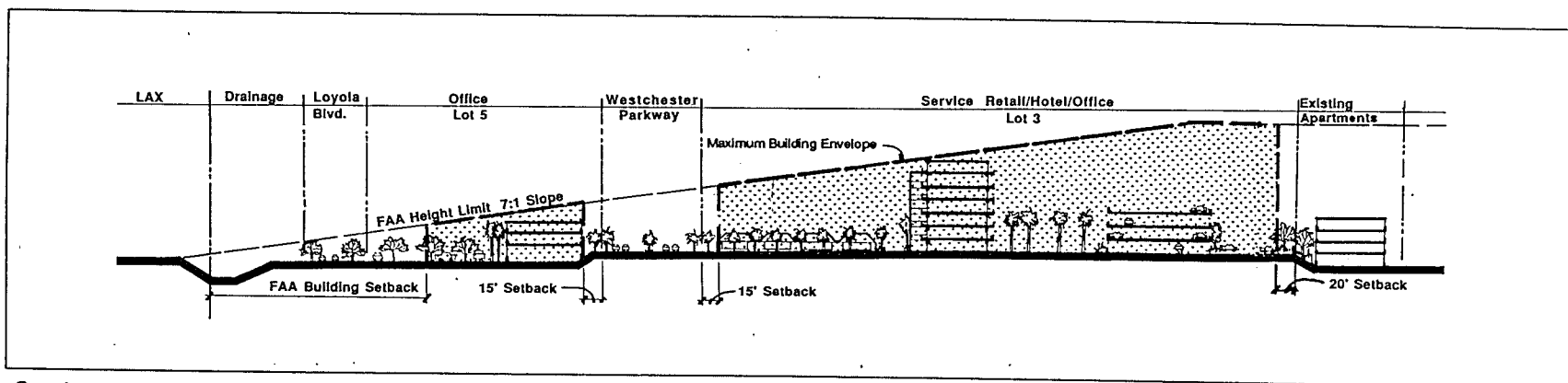


*Section 4: Between Rayford Drive and Loyola Boulevard*

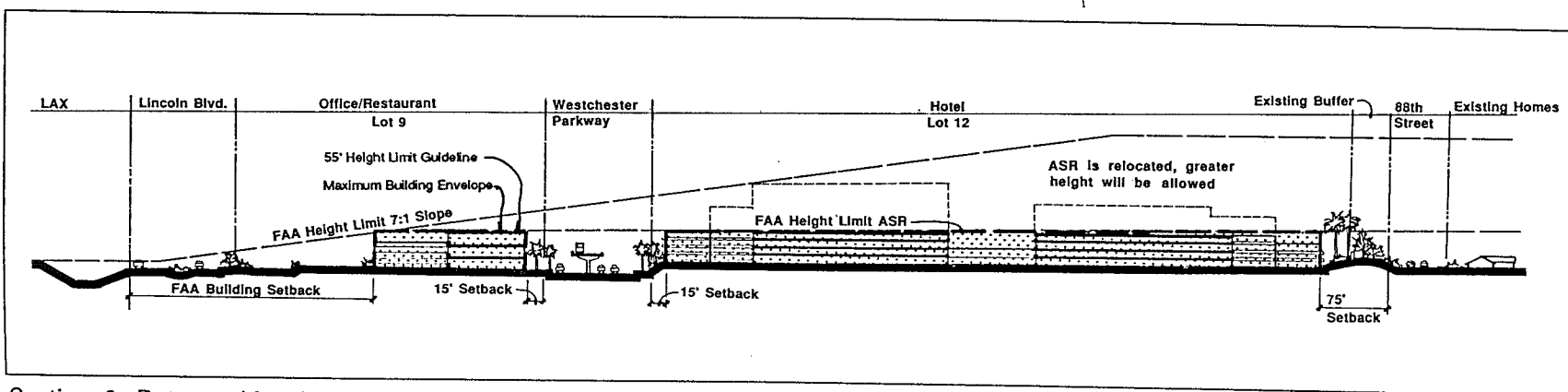
### Westchester Center Guidelines

1. In order to ensure uniformity along the streetwalls of Westchester Parkway, all structures along Westchester Parkway are restricted to a maximum height of 45 feet measured along the 15 foot setback line.
2. If greater height is desired and permitted under FAA regulations, an additional setback of at least 10 feet is required. Behind this setback, building to the maximum height allowed by the FAA is encouraged.

The greatest opportunity for individual expression are major recesses, balconies, rooftops and setbacks is above this 45 foot height limit. The changes would be primarily noticed from longer views and contribute to the skyline of the project.



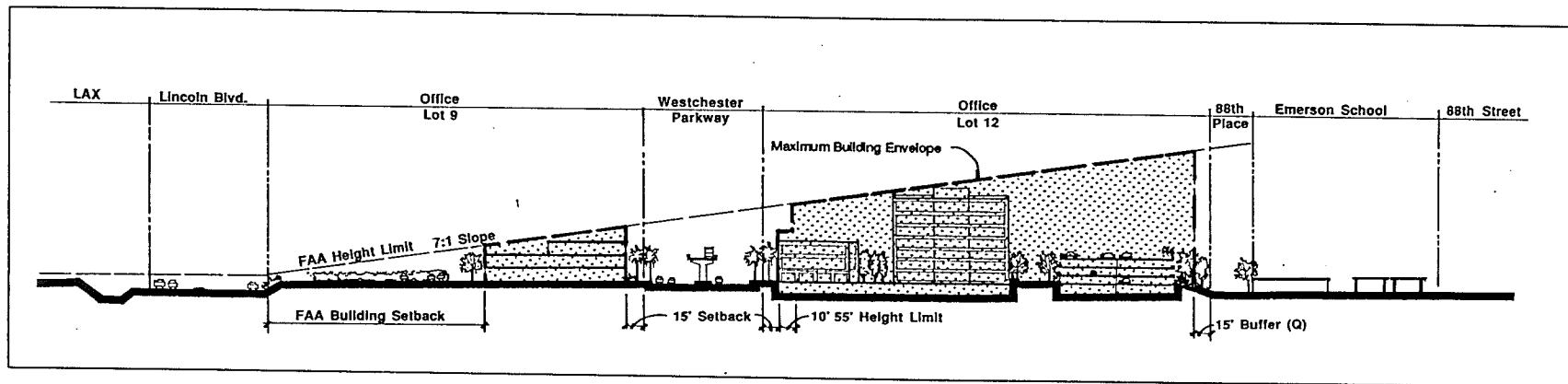
Section 5: Between Loyola and Lincoln Boulevards



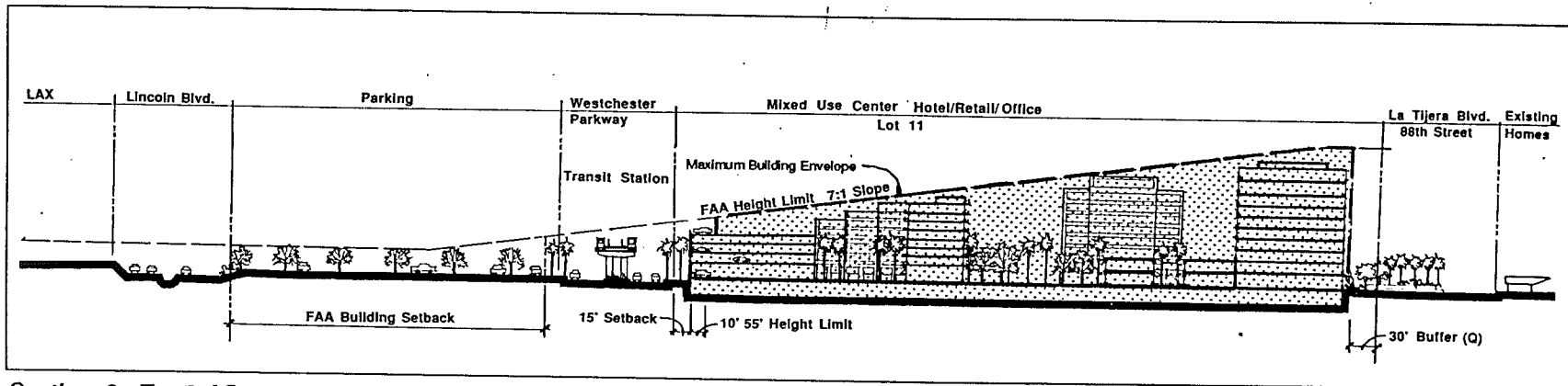
Section 6: Between Lincoln and Emerson

3. Structures south of Westchester Parkway are restricted to a maximum height of 45 feet measured at a line 15 feet back from the property line.

Heights of structures are further controlled by FAA height regulations.



Section 7: Between Emerson and Liberator Avenues



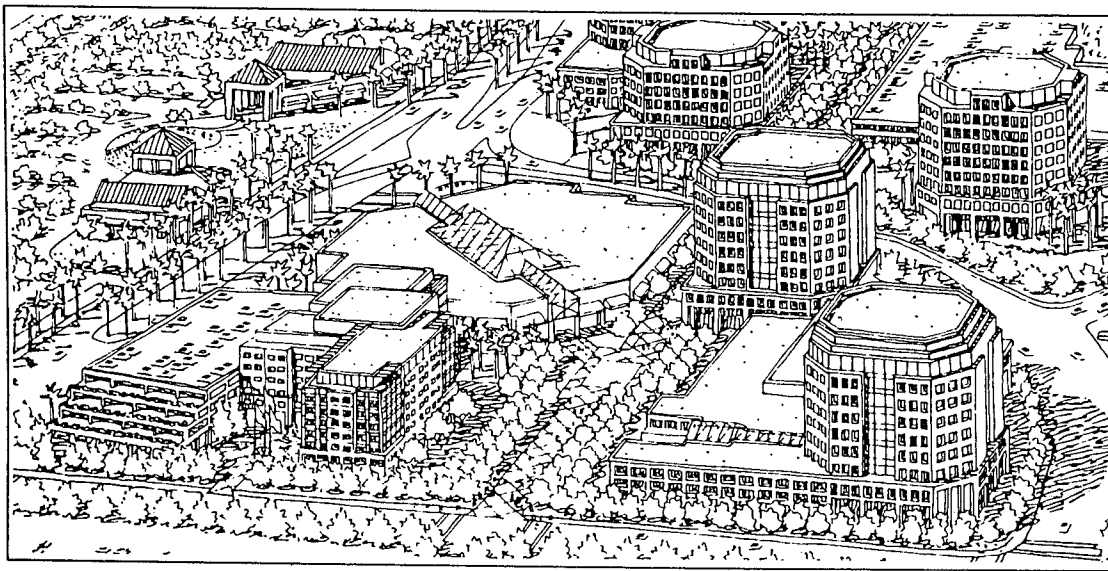
Section 8: ~~East~~ of Sepulveda Westway  
West



## 5.3 Facade Articulation

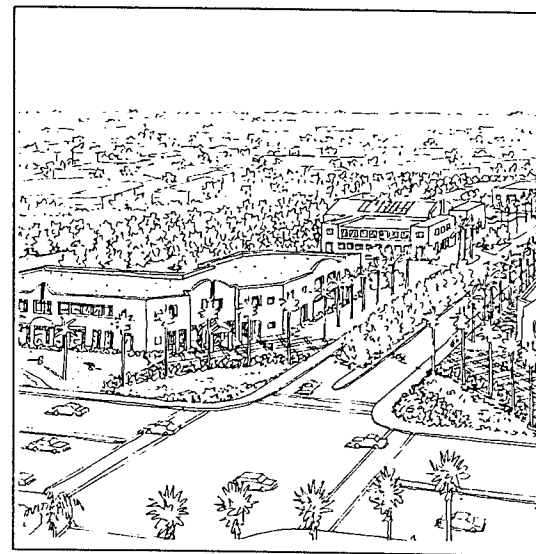
### Westchester Center Guidelines

1. On most blocks a rhythm will be set either by lot widths or the structural bay widths of buildings. Standard proportions of the buildings in the area will be determined by prevailing street wall height and building widths.
2. Because of the large lot sizes, and relatively wide and low proportions of buildings, it may be necessary to break up facades into discrete elements, to ensure that a streetscape rhythm is created with an established set of proportions.
3. To reduce conflicts between adjacent buildings and minimize contrasts between individual architectural solutions, building designs should pay attention to the boundary shared with another building. Architectural features such as strongly contrasting window types that exaggerate the changes are discouraged at boundary lines. Other architectural concerns at the boundary lines between parcels, such as the base, choice of materials and colors, should be addressed in order to assure that adjacent structures relate to each other.



### Business Park Guidelines

1. On the north side of Westchester Parkway flat, unbroken or blank wall surfaces facing the parkway are discouraged. A rhythmic breakup and three dimensional modulation of wall surfaces through windows, structural bay expression, projected structural elements and recessed wall openings is highly desirable.
2. On the south side of Westchester Parkway, west of Loyola Boulevard, facades should be generally neutral, and blank walls will be permitted.
3. Highly contrasting, graphic patterns on the facades will not be allowed.



## 5.4 Base/Street Relationships

### Background

The base of the building is generally understood to be the first or combined first and second floors and is the critical element in giving the building a positive relation to the street. It contains building entries, lobbies and areas of higher activity, and in taller buildings establishes the architectural base for the upper floors.



### Westchester Center Guidelines

1. There should be a distinctive architectural expression of the building's base utilizing techniques such as the expression of a cornice line, changes in materials or surface texture, geometry, proportions of openings, setbacks and recesses that create shadows.
2. The base should be built of permanent masonry materials such as stone, block, brick, tile or integrally colored concrete. Neither metal nor stucco are permitted as predominant materials.
3. Blank walls at ground level are discouraged along retail frontages. A maximum number of windows is encouraged.
4. Ground level windows must be transparent. Very dark or mirrored glass is prohibited within the base of the building.
5. Sun shielding should be accomplished architecturally either by recessing or by means of awnings.
6. Buildings' ground floor levels should match at the adjoining sidewalk level.

## 5.5 Materials/Colors

### Background

Light colors are effective in reflecting light, reducing heat gain in buildings and make an effective backdrop for landscaping. For these reasons lightly colored buildings tend to dominate in southern, warm climates such as that of Los Angeles.

### Objectives

- To establish a palette of materials and colors which is somewhat consistent and responsive to the climate.
- To minimize marked contrasts between adjacent buildings.

### General Guidelines

1. The predominant colors of exterior building materials should be light in tone. Neutral, warm earth tones and pastels of very light value are preferred.
2. At the ground level bright accent colors associated with pedestrian activity are encouraged.
3. Mirror glass and highly reflective surfaces will not be allowed as dominant building materials.
4. Tilt-up or pre-cast concrete with integral color, and metal panels with a matte silicone polyester or Kynar finish are desirable.

## 5.6 Roofs

### Background

Most of the building will be low, with large expanses of roof areas. Roof design becomes important when mechanical equipment located on them might be visible from adjacent property or when the roofs themselves are visible from nearby residential buildings at a higher grade or from nearby, taller buildings.

### General Guideline

1. All mechanical roof top equipment should be screened from view from nearby streets and property.

### Westchester Center Guideline

1. The roofs of the lower buildings within *Westchester Center* should be interesting to look at from nearby, taller buildings.

### *KA* Business Park Guideline

1. The roofs of buildings in Lots 1 and 4A shall be specifically designed to minimize impact on views from nearby residences.

## 5.7 Parking Structures

### Background

Parking structures will be designed to the highest standards of quality, and should comply with the other architectural guidelines in this section, but additional requirements pertain because of the nature of the building type.

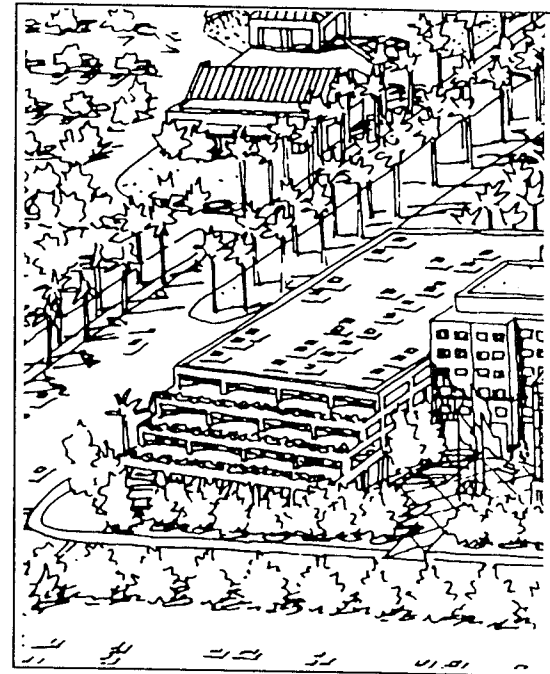
### General Guidelines

1. Sloped ramps are to be located within the structure and away from any street elevations.
2. Ground floor areas adjacent to primary pedestrian paths should be occupied with retail or service uses. Where this occurs there should be special architectural treatments of the commercial frontage.
3. Pedestrian entries to parking garages should be at ground level. Upper level connections between parking structures and the buildings they serve are discouraged.
4. Colorful canopies, awnings and porte cocheres should be used to mark both vehicular and pedestrian entries to the garage.
5. All lighting within and on the roof of the structure should be shielded so that the light sources are not visible from adjacent property or rights of way.

## 5.8 Ancillary Buildings

### General Guidelines

1. Ancillary buildings should relate in style and materials to the main buildings with which they are associated. They should be built of permanent materials.
2. Ancillary buildings, including gate houses, should conform to all setback guidelines.



## 5.9 Service, Loading and Utilities

### Objectives

- To provide service and mechanical elements in a simple, efficient way which does not interfere with the building's function.

### General Guidelines

1. Loading areas should be accommodated entirely on-site.
2. Parallel parking space for delivery trucks should be provided along service drives or in specially designated courts or loading docks.
3. Loading docks and trash storage should not be located along major street frontages and should be screened from view by walls and doors. Service elements such as loading doors should be integrated with the facade design in an organized manner.
4. Service areas should be located in a manner that does not conflict with auto or pedestrian movements.
5. No materials, supplies or equipment, including trucks or other motor vehicles, are to be stored on-site unless inside a closed

building or behind an architectural screening, except for airport-related uses located in Lot 4, out of the view of public streets.

6. Exterior utilities such as water, gas, sewage, electrical, and communication lines must be installed underground.
7. All mechanical equipment, utility meters, storage tanks, air conditioning equipment, and similar equipment shall be screened from view by landscaping or attractive architectural features integrated into the structure, except that in lot 4, such equipment located on the backside of buildings, and out of the view from public streets, need not be screened.

## 5.10 Exterior Walls and Fences

### Background

Along the north property line walls and fences are required as part of the buffer system, but elsewhere they will not be needed except to screen specific areas of uses, define outdoor courtyards, or for security of airport-related uses and the airport.

### Objectives

- To design walls and fences which integrate with and complement the building architecture.
- To avoid interrupting the continuity of the linear open space along the parkway.

### General Guidelines

1. No walls or fences shall be constructed within the building setback along Westchester Parkway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue or Pershing Drive.
2. Walls and fences will not be permitted along interior lot lines, except in Lot 4.
3. Perimeter fences and walls required by the zoning conditions shall be provided. (See Section 6.0, Landscape Guidelines.)

# **6.0**

## **LANDSCAPE GUIDELINES**



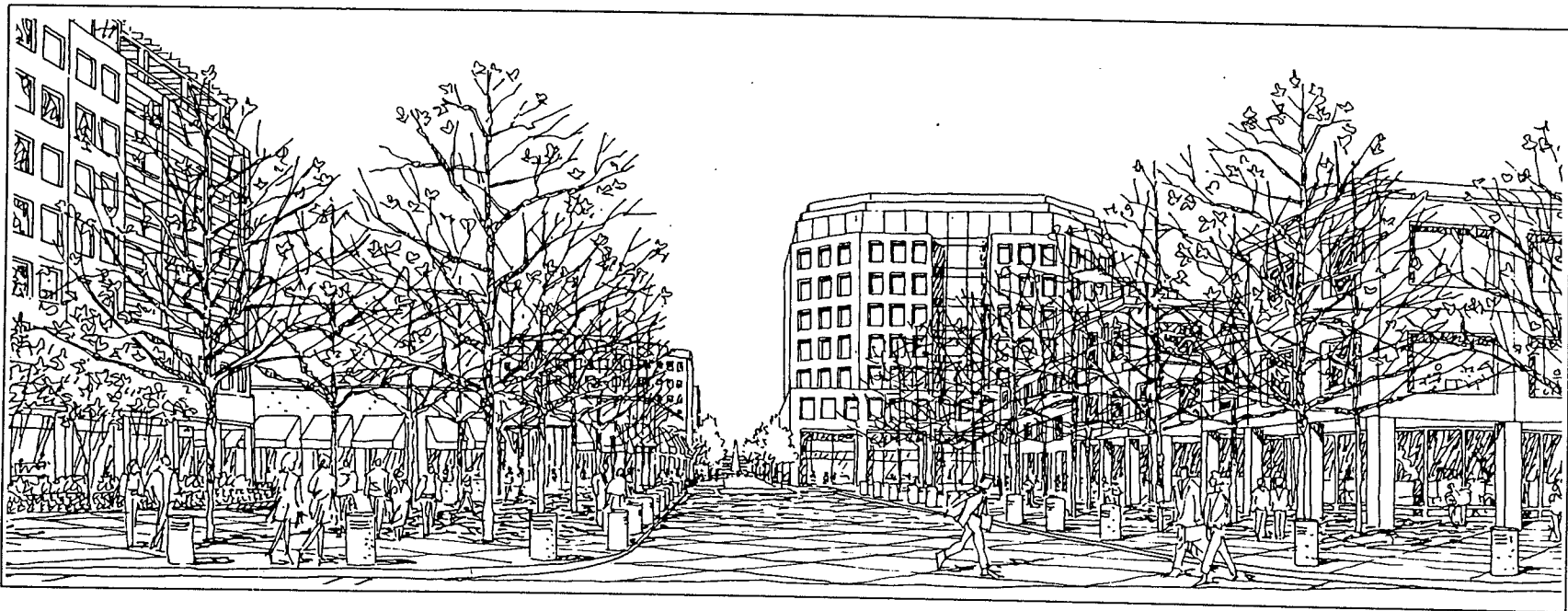
The "Landscape" as described in this section refers to all those elements that give form to the exterior environment of LAX Northside business development. These include such items as streets and building setbacks, the variety and placement of plant materials, walkways, signing, lighting, site furnishings, and arrangement of major functional elements including project entries, parking lots, buildings, service areas, etc. The objective of these Landscape Guidelines is to ensure that while project architecture and building sites within

the business community vary in type and size, landscaping as a design element will play the key role in creating and conveying the overall character of *LAX Northside* development.

The landscape design and guidelines support the overall development concept of the two zones, as defined in Section 1.3. The first zone is *Westchester Center* in which the landscape exhibits an urban character. The second zone is located on the western portion of the site and is called the *Business Park*. The concept for

the *Business Park* is to create a "parklike" suburban working environment. The two zones, although contrasting, will be linked together by the street landscaping and the landscape buffer on the north property boundary.

The landscape guidelines are organized into three prominent landscape areas: 1) Public Streetscape, 2) Common Areas, and 3) Parcels. Following each section of the three landscape areas will be a selected list of plant material which is appropriate to that zone.



*Street Landscaping*

## 6.1 Public Streetscape

The Public Streetscape includes street medians and parkways; these elements form the overall project area image. Within *LAX Northside* development there are eight streets which have general and specific landscape requirements. The eight streets are:

1) Westchester Parkway, 2) La Tijera Boulevard, 3) Falmouth Avenue, 4) Pershing Drive, 5) Loyola Boulevard, 6) Lincoln Boulevard, 7) Sepulveda Westway, and 8) Emerson Avenue. Each street will address, if applicable, the requirements for the landscape median and the 10' landscape parkway.

### General Guidelines

1. **Tree Grates:** Tree grates shall be cast iron, as approved by the City of Los Angeles standard.
2. **Concrete Sidewalks:** Concrete walks shall be ten feet (10') wide, per City of Los Angeles standard.
3. **Handicap Ramps:** Handicap ramps shall be provided at all locations where sidewalks meet street corners or driveways and shall be designed consistent with Federal, State and City handicap requirements. Refer to the Los Angeles City Standard.
4. **Street Trees:** Street trees shall be spaced per City of Los Angeles Street Trees Standard available through Public Works. Designated street trees are indicated below.

### Westchester Center Guidelines

#### 1. Westchester Parkway at Westchester Center:

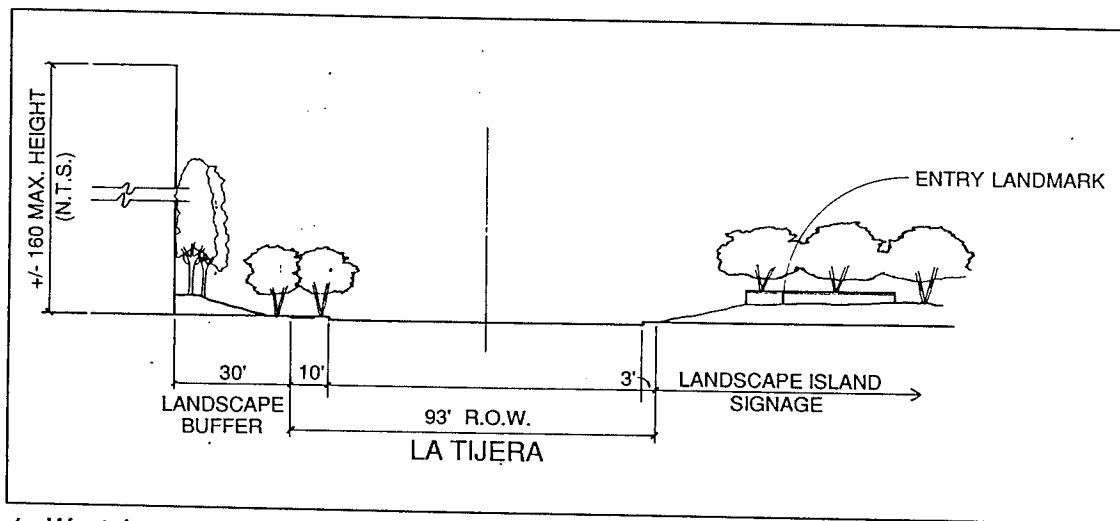
- a) A single row of Queen Palms shall line both sides of the street @ 40 feet on center. The palms shall be located behind the back of the curb with a tree grate.
- b) The median shall be landscaped with a double row of Canary Island Date Palms.
- c) The median shall have a 5:1 maximum slope berm with turf.
- d) See figure 1.

#### 2. La Tijera Boulevard:

- a) A single row of Carrotwood trees shall be planted @ 40 feet on center, on both side as illustrated.
- b) See "Common Areas" section for the landscape requirements of the 30 foot landscape setback buffer and La Tijera gateway.
- c) See figure 2-4.

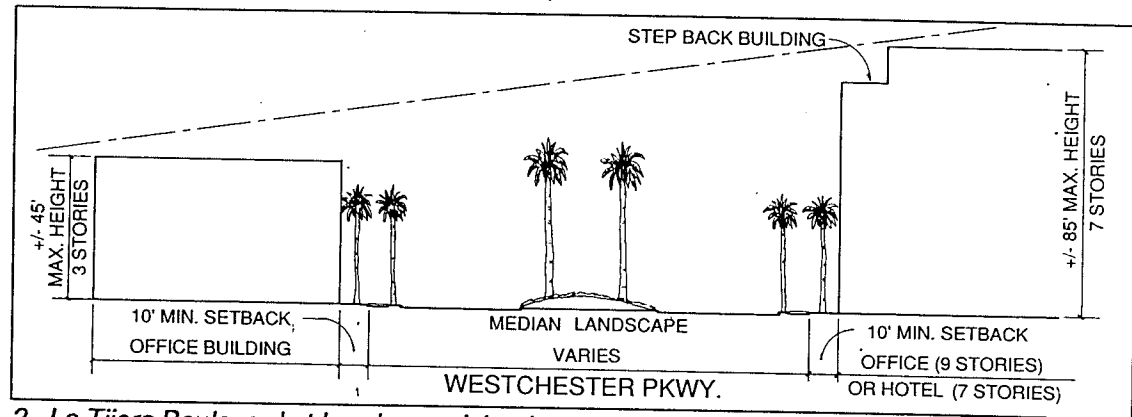
#### 3. Sepulveda Westway:

- a) A single row of Magnolia Grandiflora Majestic Beauty shall be planted on each side of Sepulveda Westway.
- b) See figure 5 for sectional view.

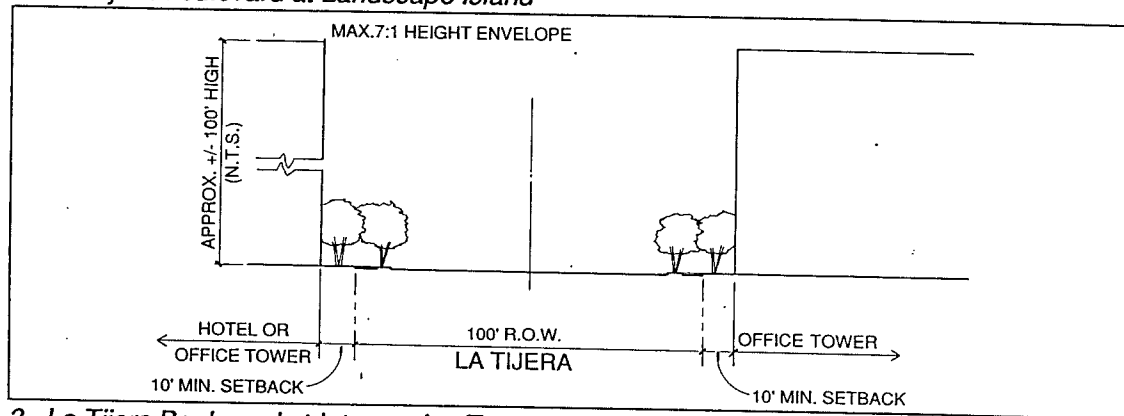


1. Westchester Parkway at Westchester Center

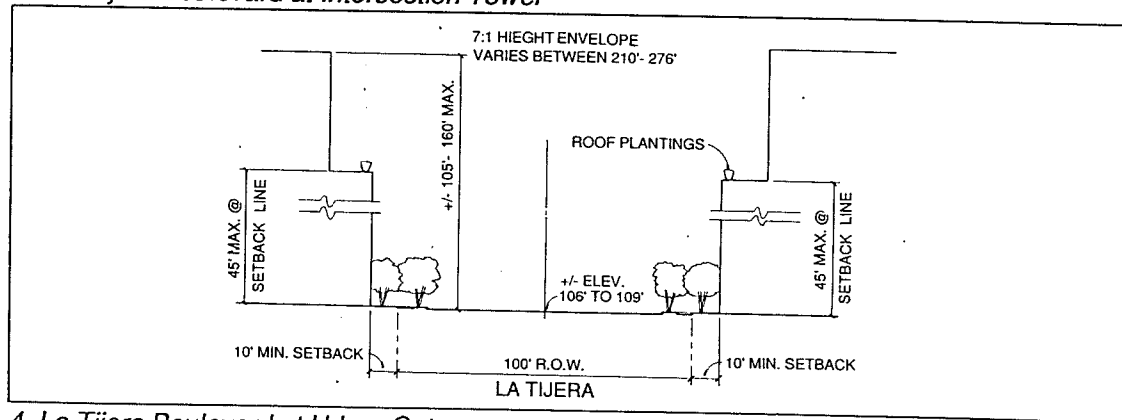




2. La Tijera Boulevard at Landscape Island



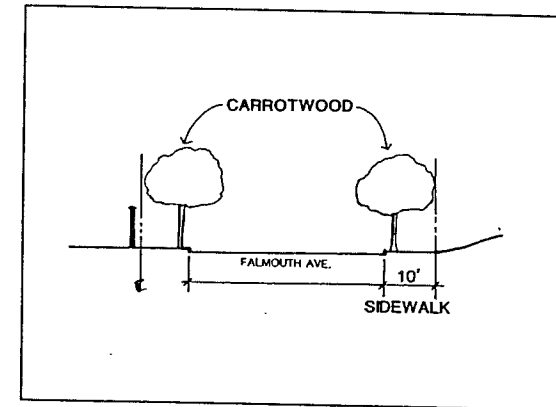
3. La Tijera Boulevard at Intersection Tower



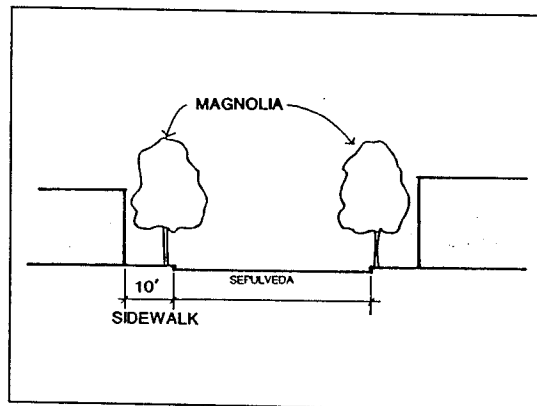
4. La Tijera Boulevard at Urban Gateway

## Business Park Guidelines

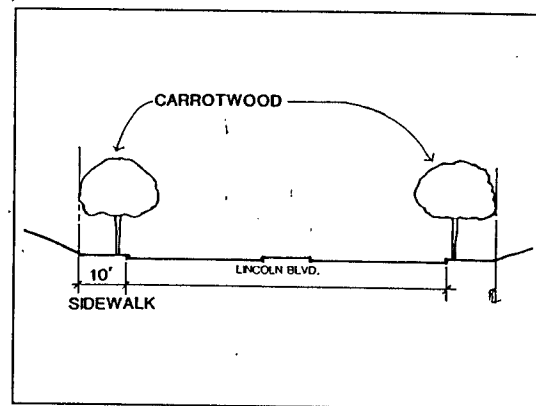
1. Westchester Parkway at the *Business Park*
  - a) A single row of Queen Palms shall be placed adjacent to the curb on both sides of LAX Northside at 40' on center.
  - b) The landscape median shall be planted with alternating rows of Canary Island Date Palms and Coral Trees.
  - c) A row of shrubs shall be located down the center of the median with Hedera Helix Needlepoint as the common ground cover. The row of shrubs shall be low varieties of Nerium Oleander and Raphiolepis Indica.
  - d) See figure 5.
2. Lincoln Boulevard and Loyola Boulevard:
  - a) A single row of Carrotwood trees shall be planted on each side of Lincoln Boulevard and Loyola Boulevard.
  - b) See figure 6 for sectional view.
3. Falmouth Ave
  - a) The street tree shall be Carrotwood tree.
  - b) See figure 7 for sectional view.
4. Pershing Drive
  - a) On the east side of the drive shall be a single row of Ficus Microcarpa at 40 feet o.c. per city standard.
  - b) See figure 8 for sectional view.



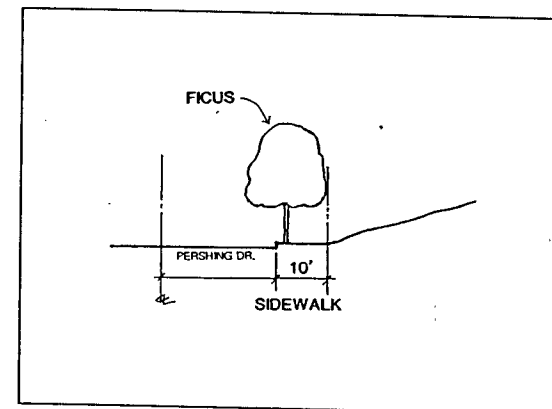
7. Falmouth Avenue



5. Sepulveda Westway



6. Lincoln Boulevard



8. Pershing Drive

## 6.2 Common Areas

The "Common Areas", as defined in the guidelines, are key visual landscape areas that give the project identity when one arrives by air and drives through the project. The elements of the Common Areas include:

1) Landscape Buffer Setback, 2) Gateways, and 3) Primary and Secondary Intersections. (See Figure 9).

### Landscape Buffer Setback

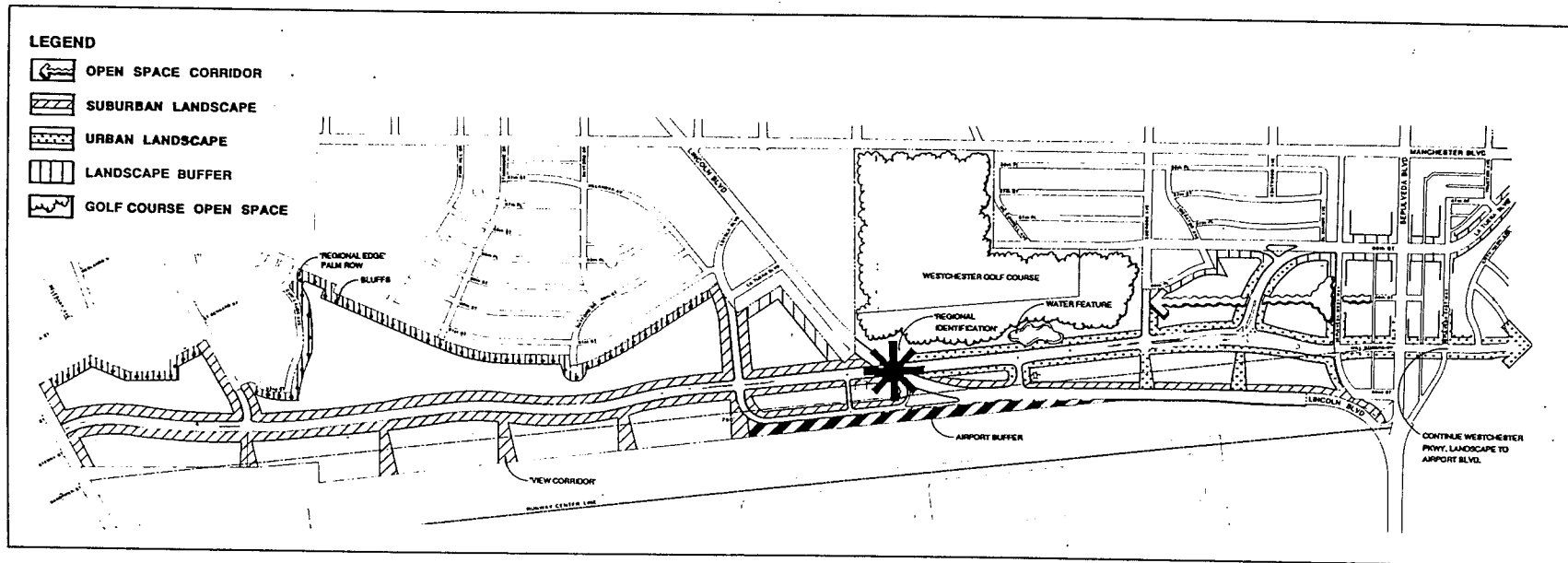
Landscape buffer setbacks are a transition zone which screen and buffer the residential uses from the project development. The buffers reflect the implementation of the project's zoning requirements.

### General Guidelines

1. Since the common area landscape will not be installed at one time, each project shall adhere to the guidelines set forth and if a project is already in place, the new project should design its landscape to relate to it. The landscape should appear to be continuous and uniform.
2. Refer to Figure 9 for landscape setback requirements.

### Westchester Center Guidelines

1. Landscape Buffer Setback at Parcels 11 and 12A along La Tijera Boulevard and 88th Street:
  - a) A 30 foot wide landscape buffer set back along 88th street between Sepulveda Westway and Liberator Avenue shall be required. No building or structures are allowed within the setback.
  - b) Screen walls are permitted, but shall not exceed 6 feet in height from street elevation. Walls and fences shall integrate with architectural design theme.



9. Open Space Diagram

- c) Landscape planting shall be informal massings of trees and shrubs that screen surface parking and parking structures and reduce the perceived scale of development.
- d) See Figure 10 for sectional view.

2. Parcel 12A at 88th Place and Liberator Avenue:

- a) A 15 foot wide landscaped buffer set back along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site) shall be planted with a dense row of evergreen screen trees. The trees should screen visibility into the site.

- b) A 6 foot high chain link fence is required two feet behind the property line. This two foot area shall be landscaped with massings and layers of shrubs, vines and ground cover.
- c) See Figure 11 for sectional view.

Business Park Guidelines

1. Parcel 2 along north property line:

- a) A 15 foot landscape buffer setback along the north airport property line between Loyola Boulevard and Hastings Avenue shall be planted with dense trees, tall shrubs and ground cover immediately south of the property line.
- b) Where the landscape buffer is on a slope condition, the proposed planting should effectively control erosion problems.
- c) An 8 foot high masonry block wall is required along the north property boundary. The wall shall be measured in height from the finished grade of the alley between Loyola Boulevard and Rayford Drive. The masonry wall shall be constructed out of slump stone. The color shall be light in value.
- d) See Figure 12 for sectional view.

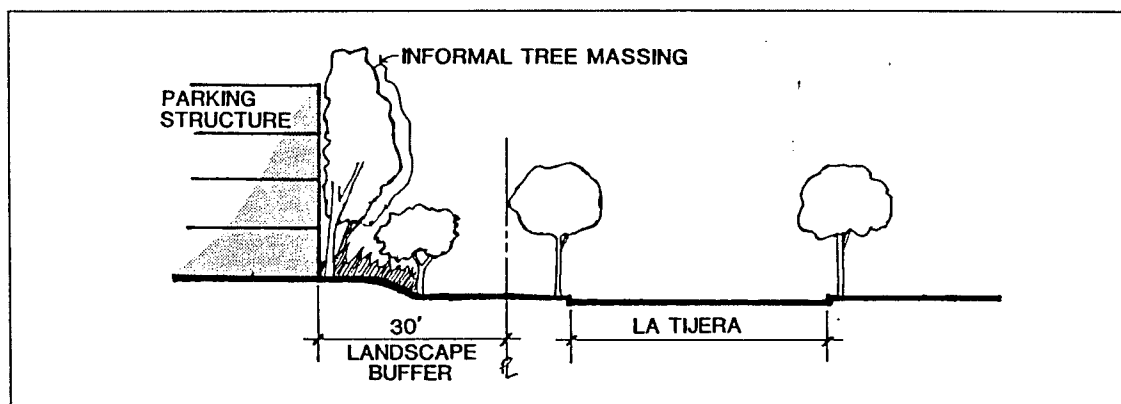


Figure 10

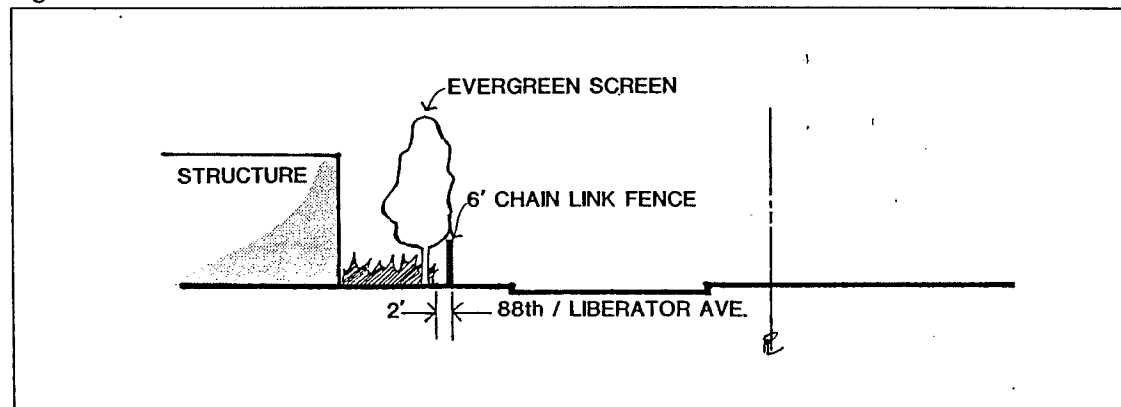


Figure 11

2. Parcel 2 along Cum Laude Avenue and 92nd Street:

- a) A 10 foot landscape buffer along the property line shall be planted with a dense informal grove of evergreen trees. The visibility into the site from this edge shall be screened.
- b) A 6 foot high chain link fence is required along the property line. The fence area shall be heavily planted with shrubs, vines and ground cover.
- c) See Figure 13 for this sectional view.

3. Parcel 1 along the north boundary between Falmouth Avenue and Pershing Drive:

- a) The 50 foot wide landscaped buffer setback shall be planted with low shrubs and ground cover that will not obstruct views from the residential units.
- b) A 6 foot high chain link fence is required along the property line.
- c) See Figure 14 for this sectional view.

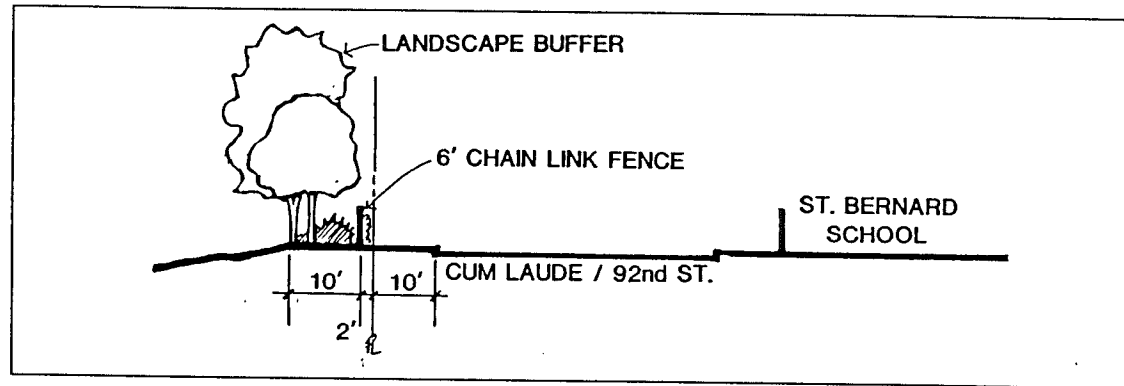


Figure 12

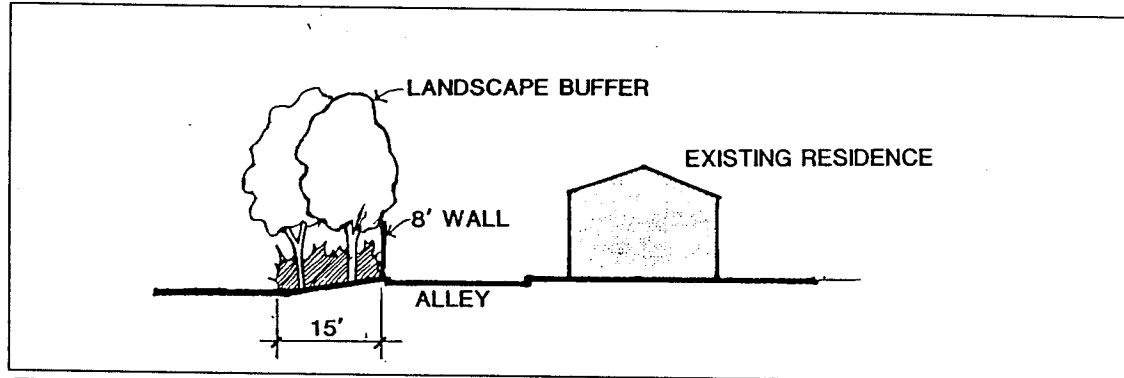


Figure 13

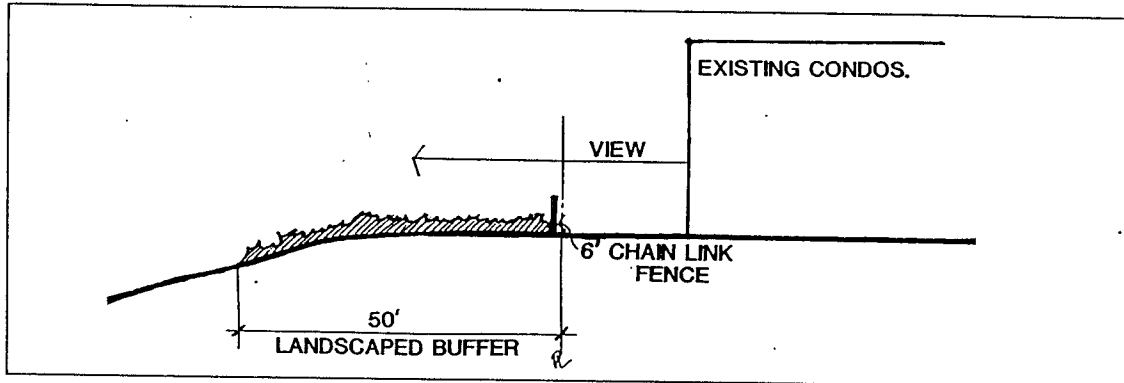
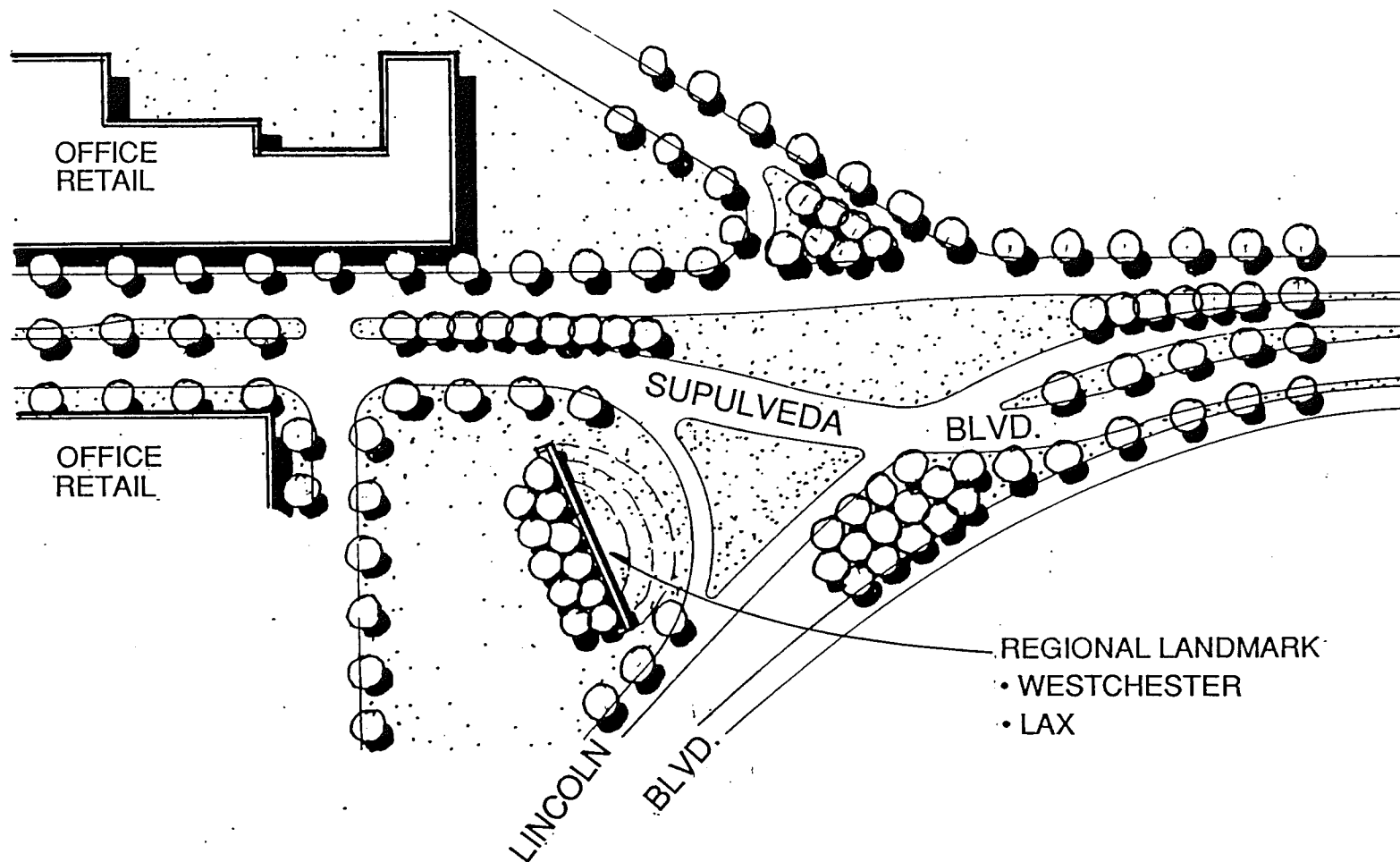


Figure 14



*Regional Gateway at Sepulveda*

### Gateways

The gateways into LAX Northside are located at: 1) Lot 10 Gateway @ Sepulveda Boulevard and Lincoln Boulevard, 2) La Tijera Boulevard; 3) Westchester Parkway @ Sepulveda Westway; and 4) Westchester Parkway @ Pershing Drive.

### General Guidelines

1. Each gateway will include project identification and directional signing. See section 6.0 for general and specific signing guidelines.

### Westchester Center Guidelines

1. Lot 10 Gateway at Sepulveda Blvd and Lincoln Boulevard
  - a) The entry trees shall be Coral Tree.
  - b) The sign wall, as shown in the signing guidelines section, shall be a large curvilinear wall which is integrated into the landscape berm.
  - c) The sign wall shall be visible from vehicles traveling north on Sepulveda Boulevard and Lincoln Boulevard
  - d) See Figure 16 for plan.
2. La Tijera Gateway:
  - a) The gateway shall be located on the island at the intersection of 88th Street and La Tijera Boulevard.
  - b) Coral Trees shall be planted in a grove behind the sign wall.
3. Westchester Parkway at Sepulveda Westway:
  - a) The sign walls shall be located on the northwest and southwest corners of the intersection.
  - b) The planting in front of the sign wall shall be a flowering ground cover. A hedge shall be planted behind the wall.
  - c) Colored concrete paving shall be placed at the crosswalks on all four streets and extends further into *Westchester Parkway* on the west side.
  - d) See Figure 17 for plan.

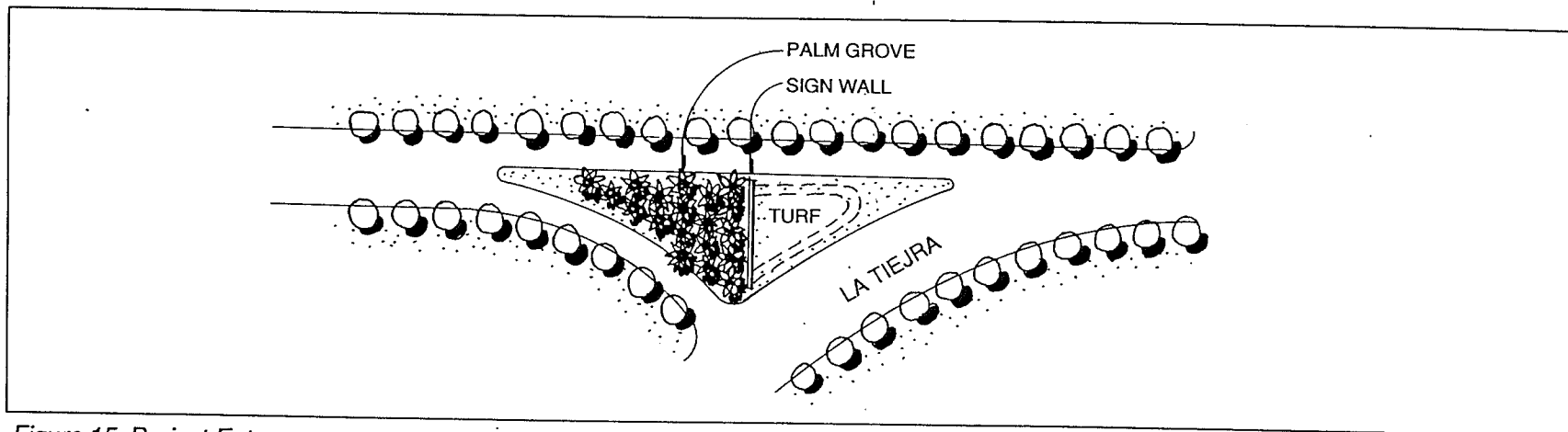


Figure 15 Project Entry

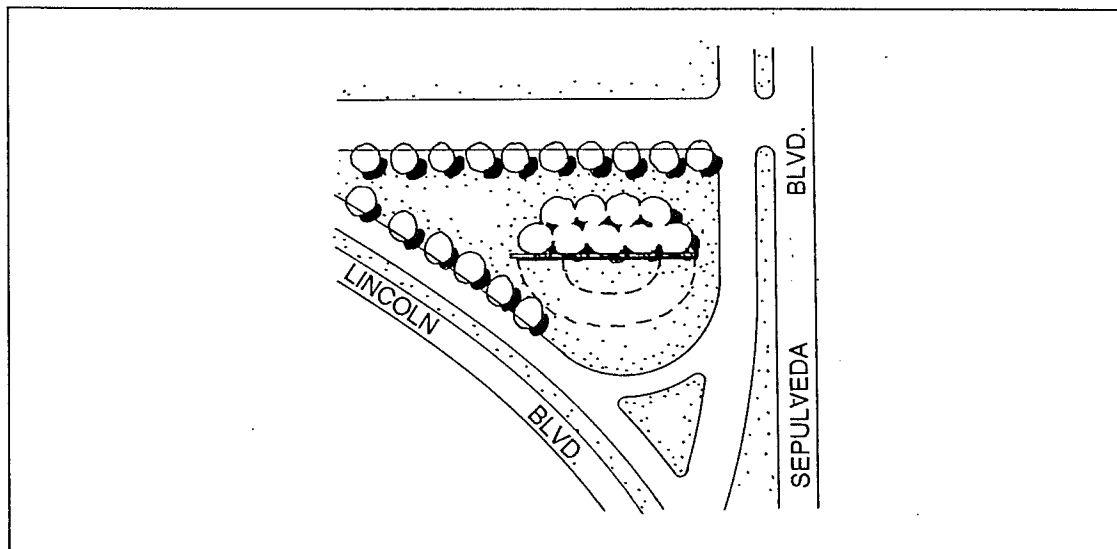


Figure 16 Project Entry

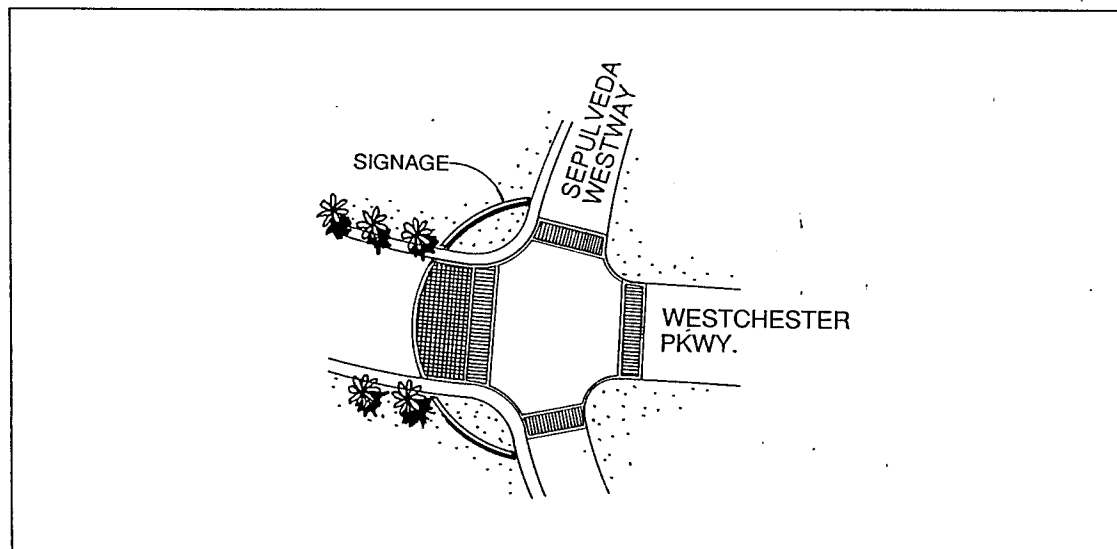


Figure 17 Project Entry Intersection

### Business Park Guidelines

#### 1. Westchester Parkway at Pershing Drive:

- a) A double row of Canary Island Date Palms forming a semi-circle is located at the entry.
- b) A 6 foot concrete walkway shall be placed between the double row of Canary Island Date Palms.
- c) The toe of the slope shall fall 5 feet behind the outside ring of the Canary Island Date Palms.
- d) The planting area inside the semi-circle shall be turf.
- e) The crosswalk shall be colored concrete paving, and shall extend up to the semi-circle area of palms.
- f) The signing shall be located per signing guidelines, Section 9.0
- g) See Figure 18.



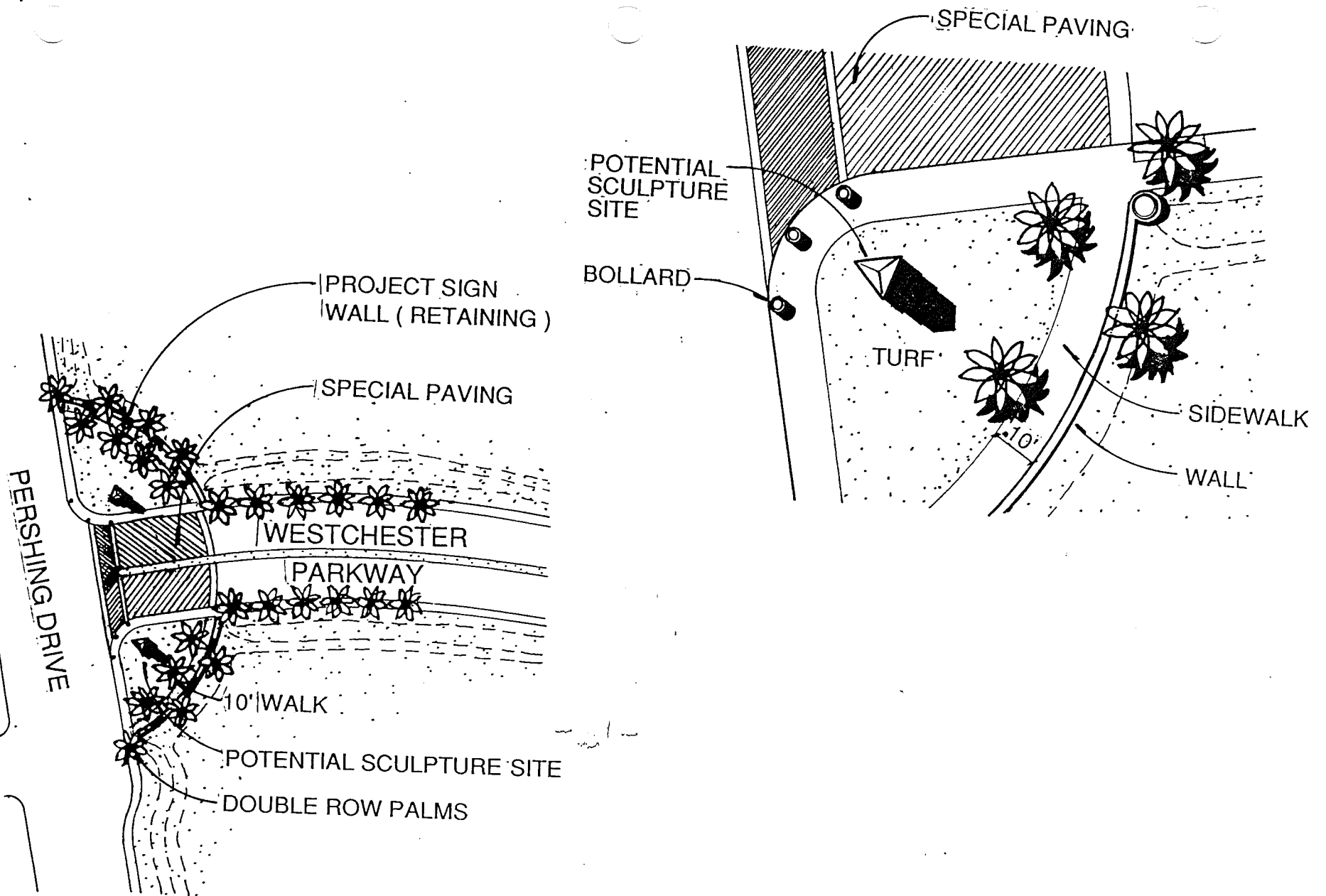


Figure 18 Entry at Pershing Drive

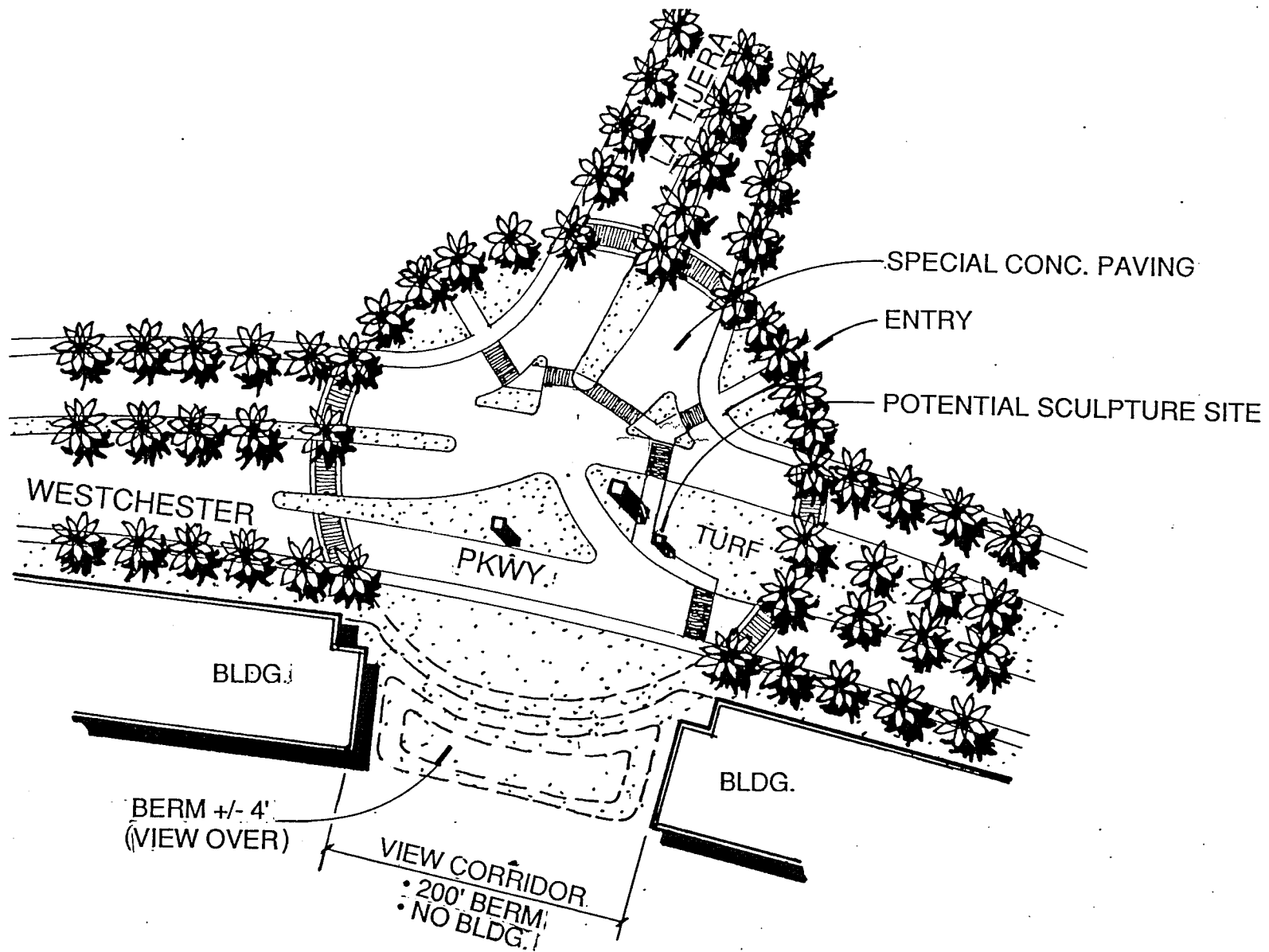


Figure 19 Urban Node

### Primary and Secondary Intersection

The primary and secondary intersections are nodes of activity and vehicle arrival points. The primary intersections are Westchester Parkway and La Tijera Boulevard, and Westchester Parkway and the overpass at Lincoln Boulevard. The secondary intersections include the major parcel entries, as well as the intersections of Westchester Parkway at Loyola Boulevard and Falmouth Avenue.

### General Guidelines

1. All colored concrete paving in roadway shall meet the Los Angeles Public Works Highway Standards.
2. The color of the concrete shall be L.M. Scofield C-11 Desert Sand Chromix add mixture or equal. The finish shall be a heavy broom finish.
3. All colored concrete shall have an 18" natural concrete band as a separation from asphalt paving.

### Westchester Center Guidelines

1. La Tijera and Westchester Parkway intersection:
  - a) A single row of Canary Island Date Palms shall ring the space. For a focal point a sculpture or identification landmark shall be placed in the median.
  - b) See Figure 19 for plan view.
2. Westchester Parkway bridge and Lincoln Boulevard:
  - a) A double circular row of Canary Island Date Palms shall be placed surrounding the intersection.
  - b) The grading shall conform to the ring of palms.
  - c) See Figure 20 for plan and perspective views.

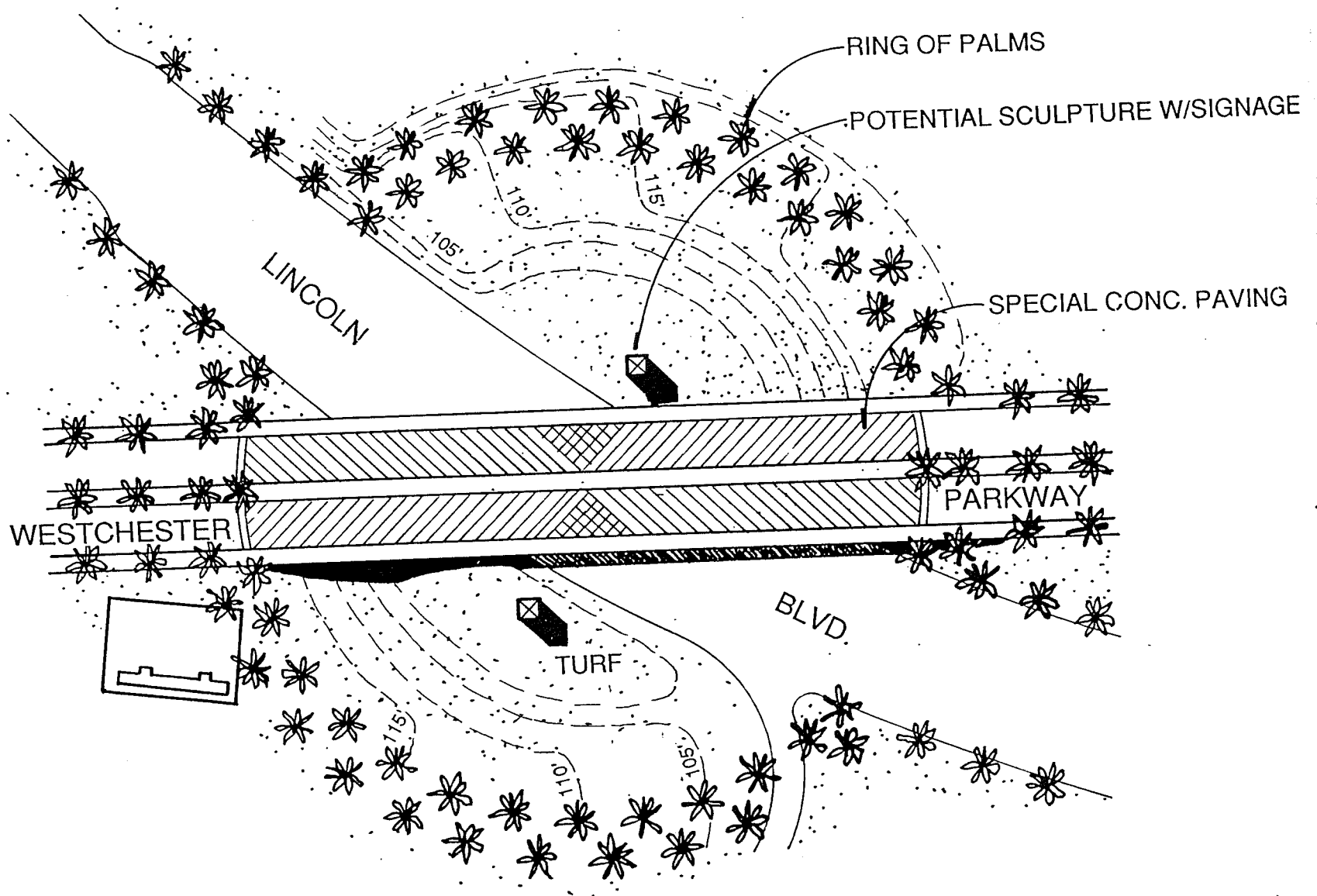


Figure 20 Bridge

### Business Park Guidelines

#### 1. Major parcel entries:

- a) The parcel entries shall have a landscape median planted with a low flowering ground cover.
- b) The entry sign shall be placed in the center of the median. The height should allow visibility over the sign to oncoming traffic.

- c) The crosswalk shall be colored concrete paving.
- d) Handicapped ramps at corners shall be required.
- d) See Figure 21 & 22, plan view.

#### 2. Westchester Parkway at Loyola Boulevard and Falmouth Avenue:

- a) The crosswalks shall be colored concrete paving.

- b) The handicapped ramp at the crosswalks shall be installed per City of Los Angeles standards.
- c) See Figure 23 for plan and sectional views.

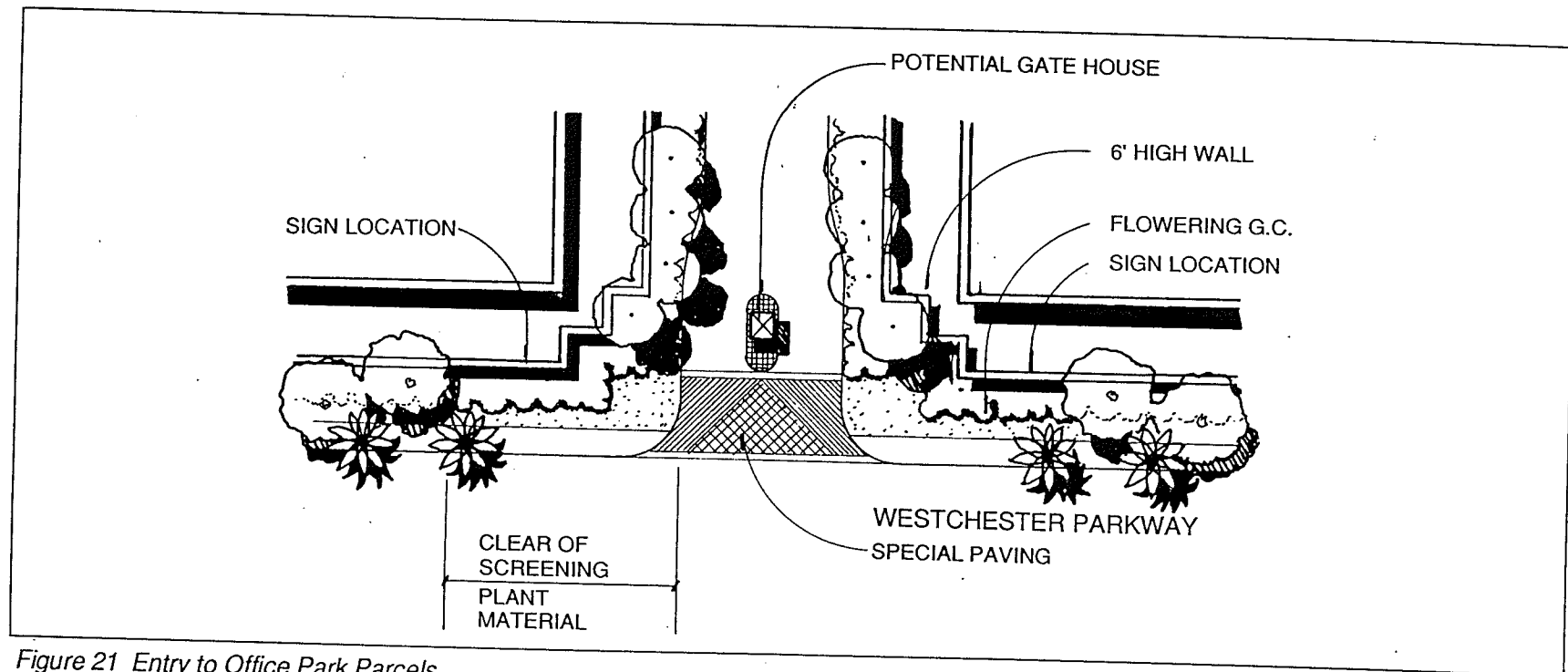


Figure 21 Entry to Office Park Parcels

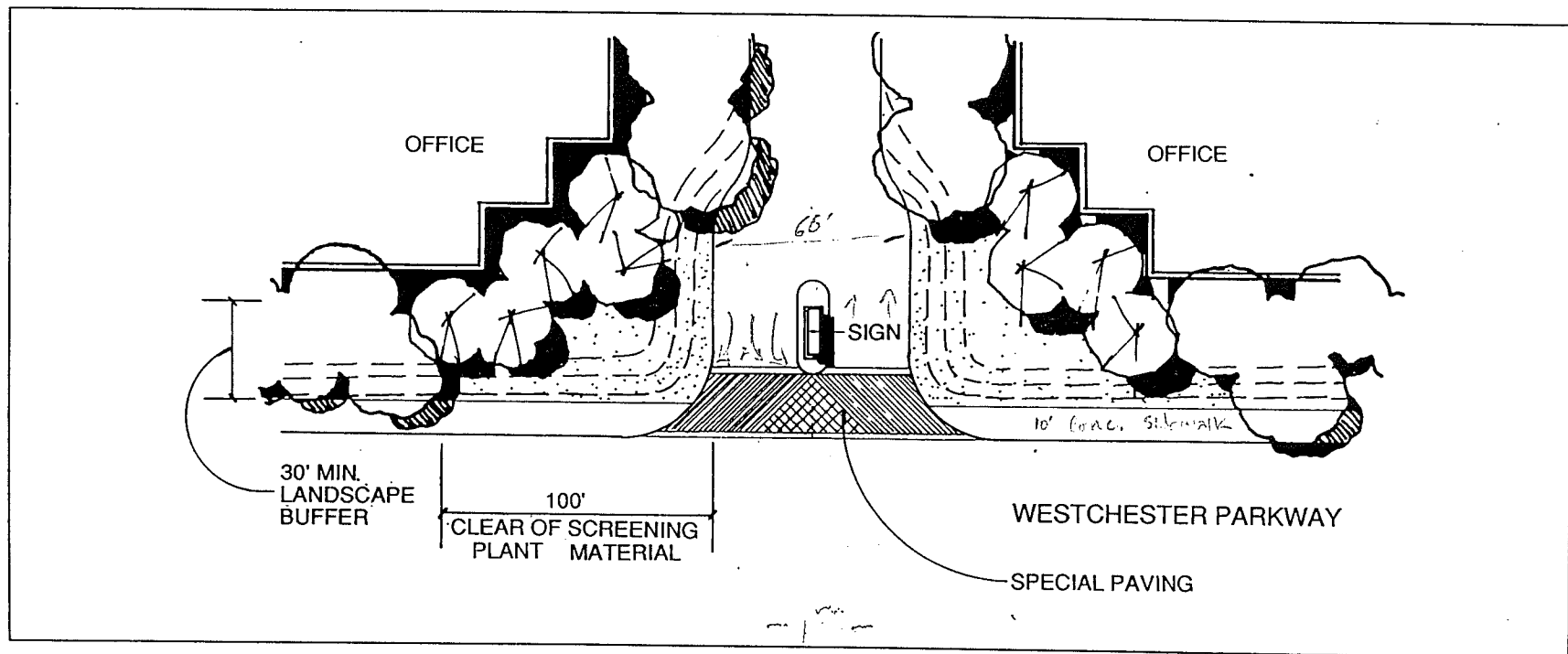


Figure 22 Entry to Airport Related Parcels

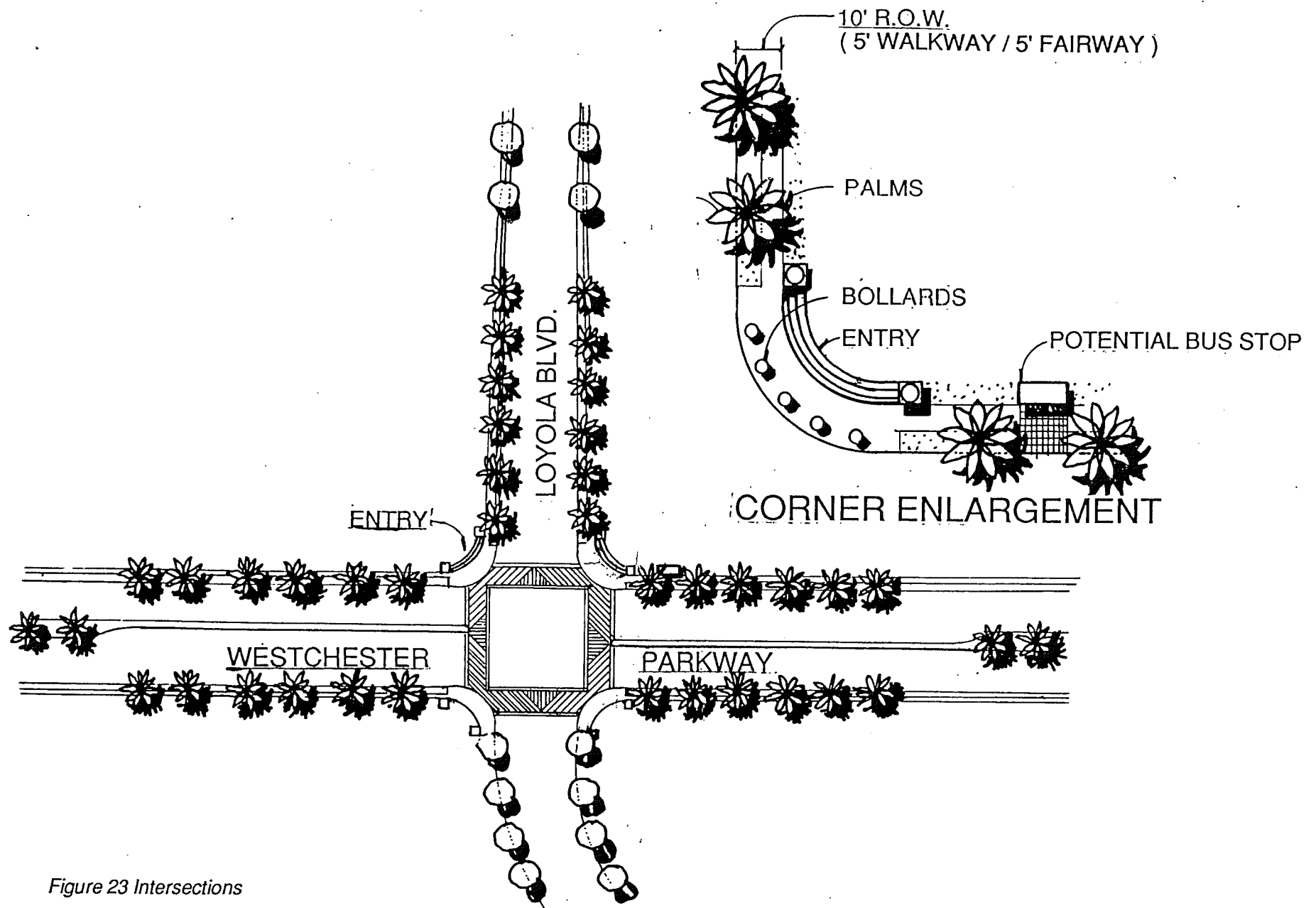


Figure 23 Intersections

## Parcels

The parcels encompass all site elements except the building within individual parcels. This area is subject to the needs of each individual tenant and thus has a greater degree of flexibility. Elements within the parcels include: parking lot arrangements, landscaping, site lighting, regulatory and directional signing, service area screening, and side and rear property line treatment.

The parcels reflect the overall development concept. (See Section for plan) The *Westchester Center* zone exhibits an urban character with formal groves of shade trees, pedestrian and vehicular plaza areas, public fountains, sitting areas, landscaped pots and accent lighting at building entrances. The concept for the *Business Park* exhibits a suburban character with large shade trees, flowering trees, large massing of shrubs, broad sweeps of turf areas and fountains as focal points. The parking in these zones shall be orchard-like and/or grove-like infill between hedgerows (side and rear property lines). Parking lots should be planted in geometric patterns with low canopy trees.

As identified by the Tract Map Qualification Conditions, the landscape setback buffering within the parcels shall refer to the "Common Areas" section of the guidelines for further requirements. The tree list also identifies selected trees that are allowed within this restricted FAA 7:1 height requirement area.

## Landscape Setback Areas

The planting in the building setback area is the responsibility of the parcel developers. All building setback dimensions are defined in the site planning Section 2.7.

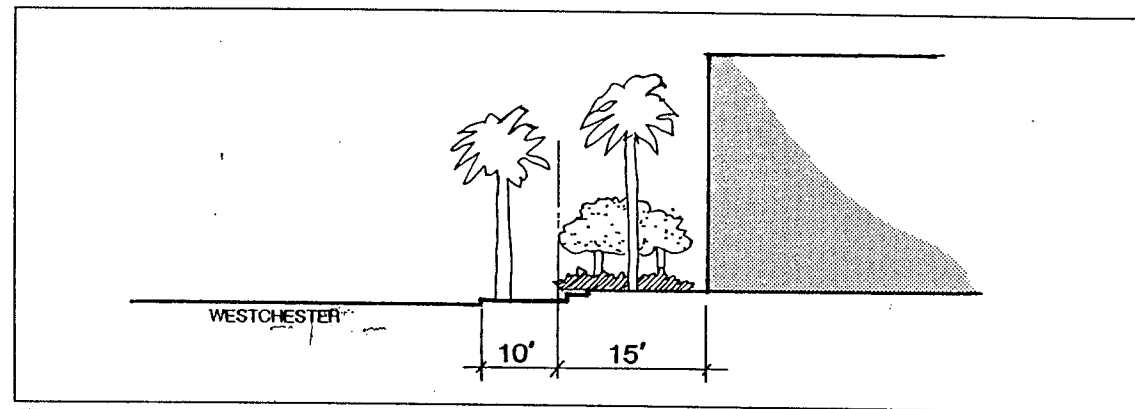


Figure 24 Westchester Parkway

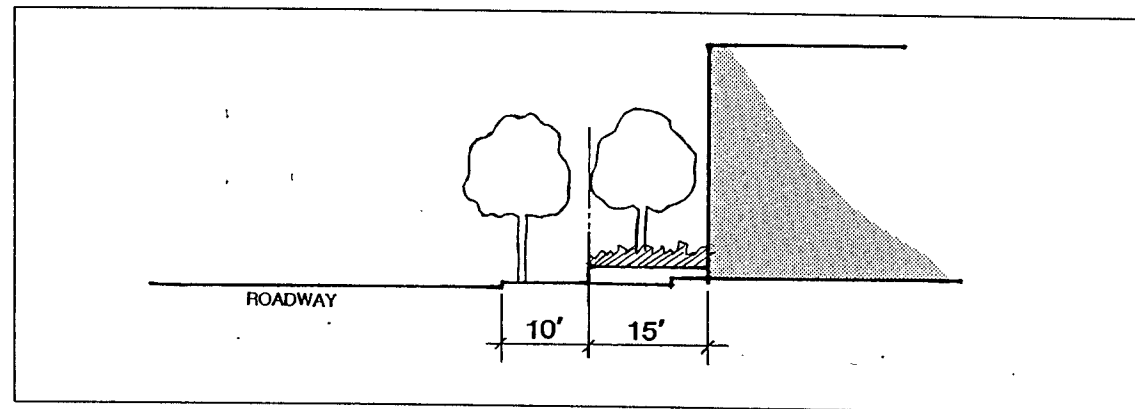


Figure 25 La Tijera, Emerson, Loyola



### General Guidelines

1. The objective of the landscape setback is to reinforce the overall development area concepts.
2. All landscape elements shall be designed and coordinated with adjacent properties so that there is a smooth transition from one property to the next.
3. Transition from the public streetscape to the parcel landscape areas shall be coordinated with grades not exceeding 2:1 and continuous planting next to the property line.
4. Surface parking areas within the building setback zone shall be a minimum of 3 feet below the top elevation of berms.

### Westchester Center Guidelines

1. Westchester Parkway, La Tijera Boulevard and Emerson Avenue.
  - a) The 15 foot building setback shall be primarily paved areas with raised planting areas for trees, shrubs and ground cover, special paving materials (ie. brick, concrete) and vertical changes where applicable (i.e., steps and walls).
  - b) A row of Queen Palms along Westchester Parkway shall be located five feet behind the property line and should align with palms in sidewalk.
  - c) See Figures 24 and 25 for sectional views.

### Business Park Guidelines

1. Westchester The Business Park of Loyola Boulevard and Pershing Drive:
  - a) Within the 50 foot building setback a 30 foot landscape area shall be planted with informal accent massing of deciduous and flowering trees with a backdrop of evergreen trees which together create a "parklike" atmosphere.
  - b) The primary landscape cover shall be turf.
  - c) The berms shall be smooth, consistent and integrated with the surrounding landscape.

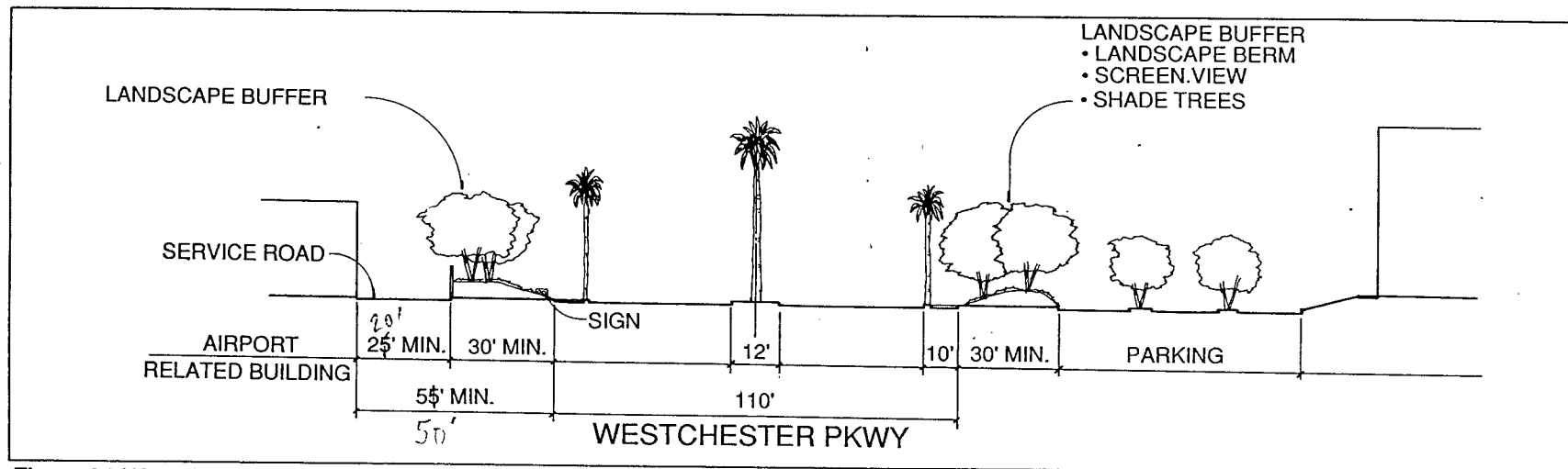


Figure 26 Westchester Parkway at Suburban Office

- d) The landscape on the south side of Westchester Parkway shall screen views such as service areas, parking lots, and airport related facilities with a 6 foot high concrete wall, turf berms and tree massing.
- e) The landscape on the north side of Westchester Parkway shall screen service areas and parking lots. The design intent is to frame views to distinctive office buildings.
- f) A four foot wide meandering jogging trail shall be located within the 50 foot setback on the north side of *Westchester Parkway*. The jogging trail shall be integrated with the turf berms.
- g) See Figures 26 and 27 for sectional views.

2. Falmouth Avenue at The *Business Park*:

- a) Within the 30 foot building setback the fifteen foot landscape setback shall be planted with evergreen trees, medium shrubs and ground cover on a 3' berm.
- b) The objective is to screen parking and service areas.
- c) See Figure 28 for sectional view.

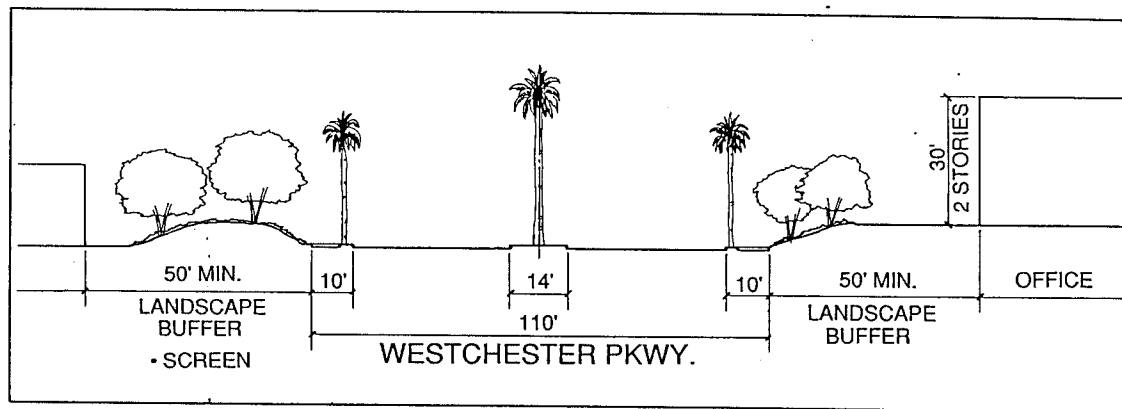


Figure 27 Westchester Parkway at Suburban office

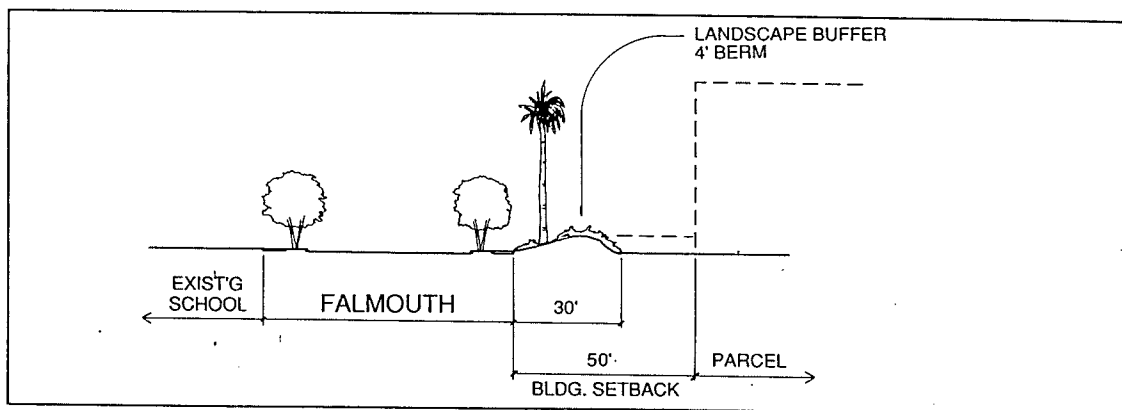


Figure 28 Landscape Buffer at Falmouth

## 6.3 Parking Lot Landscape Requirements

Landscape guidelines for parking lots shall be used for the overall development.

### General Guidelines

1. A minimum of one 24" box tree is required per each six parking stalls to be placed at the intersections and/or immediately adjacent to the stalls.
  - a) See Figure 29 for sectional view.
2. Planting between parking aisles shall have a minimum of a four foot planter area.
  - a) See Figure 29 for sectional view.
3. Tree wells and planter areas within paved parking areas shall be diamond shaped and provide a minimum 5 foot planting space.
  - a) See Figure 30 for sectional view.
4. All tree wells and planting aisles shall use curbs in lieu of wheel stops.
- 5) A single species tree is to be used for each parking compound, but the tree species may differ in separated parking compounds
  - a) Additionally a planting drainage test is recommended prior to selecting a tree species.
- 6) All parking structures shall be screened

with a 10 foot landscape buffer. The planting shall be a combination of vertical evergreen trees with medium size shrub planting at the base.

- a) See Figure 31 for sectional view.

### Side and Rear Yard Requirements

Landscape requirements for both side and rear yards shall apply to the overall development plan.

### General Guidelines

1. A 5 foot minimum planting strip is to be provided continuously along and adjacent to all interior property lines so that where two properties adjoin, there shall be a 10 foot zone.
2. A maximum slope of 2:1 is allowed within the 5 foot planting zone. A one foot minimum flat transition area shall be provided at the top and bottom of all slopes within this zone.
3. No walls or fences exceeding 30 inches in height shall be permitted within the required 5 foot planting zone.
4. Either shrub or tree plantings as specified

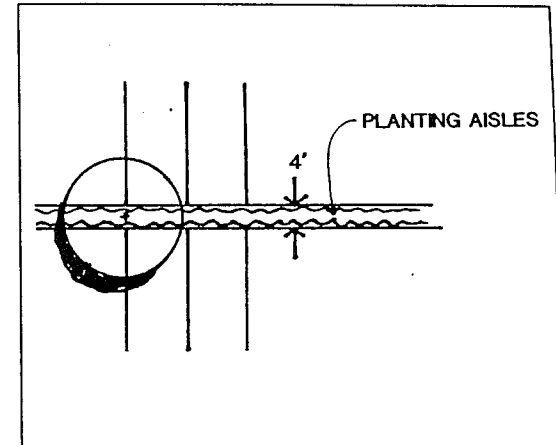


Figure 29 Container Planting

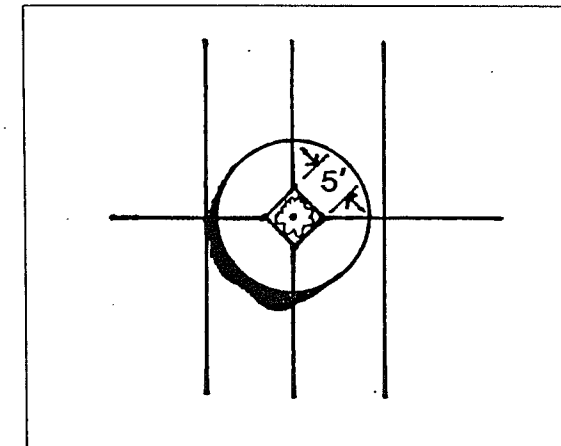


Figure 30 Planter & Wheel Stop

in the Plant Materials List shall be required in the 5 foot zone. Where two properties adjoin, shrub planting is required on one side of the property line and tree planting is required on the other. The first property developer to receive plan approval shall be required to plant the required trees. The second property developer then must plant the required shrubs.

5. Landscaping adjacent to a common access drive shall be coordinated so that both sides of the drive feature the same plant materials and include small to medium shrubs and trees on each side to screen adjacent parking. See parcel entry section of these guidelines.

### Screening Utility Service Areas

#### General Guidelines

Screening of utility service areas shall apply to the overall development concept.

1. All utility service including meters, vaults, sprinkler risers, vacuum breakers, etc. and all service and trash areas shall be screened by a combination of walls and landscape materials including trees, shrubs, vines and groundcovers.

### Planting Requirements

All landscape areas within each parcel shall be planted with materials in accordance with the planting palettes and criteria identified in these design guidelines including trees, ground cover, shrubs and vines, and such areas shall be fully irrigated with an automatic underground irrigation system, the operation of which shall be regulated by a time clock. Landscape irrigation and domestic water services to each site shall be separately metered.

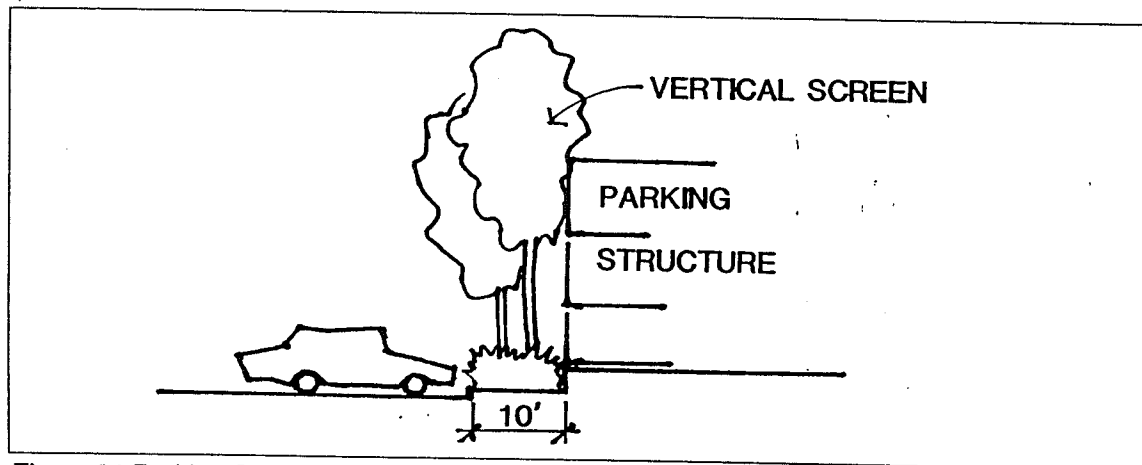


Figure 31 Parking Structure Screening

### Site Maintenance

These maintenance guidelines shall apply to individual parcel developers.

1. All areas of each lot not used for structures, walkway, paved driveways, parking or storage areas shall be at all times maintained by a professional landscape engineer or gardener in a fully and well-kept landscaped condition and according to maintenance specifications to be provided by the lessee.
2. The lessee of each lot shall at all times properly maintain and keep the entire premises in a safe, clean and sightly condition, in a good state of repair, and shall comply in all respects with all governmental, health, fire and police requirements and regulations.

3. The lessee of each lot shall, at his own expense, remove any rubbish of any character whatsoever which may accumulate on such lot.
4. Each lessee shall be responsible for the cleaning, maintenance and relamping of any external lighting fixtures excluding those street lights included in the Public R.O.W. and Common Areas.
5. Undeveloped lots shall be maintained in a weed-free condition.

### Implementation

The following matrix recommends responsibility for implementing the landscape elements described in these guidelines.

## 1. Interior Zone Tree Selection List

### A. Building Setback Landscape

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
Arecastrum romanzoffianum	Queen Palm	20' Trunk
Phoenix dactylifera	Date Palm	20' Trunk
Phoenix canariensis	Canary Island Date Palm	20' Trunk
Erythrina caffra	Kaffirboom Coral Tree	48" box
Erythrina humeana	Natal Coral Tree	48" Box
Archontophoenix cunninghamiana	King Plam	20' Trunk

### B. Parking Lot

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
Erythrina caffra	Kaffirboom Coral Tree	48" Box
Erythrina humeana	Natal Coral Tree	48" Box
Ficus microcarpa	Indian Laurel Fig	24" Box
Cupaniopsis anacardioides	Carrot Wood	24" Box
Magnolia grandiflora	Southern Magnolia	24" Box

### C. Rear and Side Yard

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
Arecastrum romanzoffianum	Queen Palm	20' Trunk
Phoenix dactylifera	Date Plam	20' Trunk
Phoenix canariensis	Canary Island Date Plam	20' Trunk
Erythrina caffra	Kaffirboom Coral Tree	48" Box
Erythrina humeana	Natal Coral Tree	48" Box
Ficus microcarpa	Indian Laurel Fig	24" Box
Magnolia grandiflora	Southern Magnolia	24" Box
Metrosideros excelsus	New Zealand Christmas Tree	24" Box
Cupaniopsis anacardioides	Carrot Wood	24" Box

#### D. Building Entries

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
Arecastrum romanzoffianum	Queen Plam	20' Trunk
Phoenix canariensis	Canary Island Date Plam	20' Trunk
Phoenix dactylifera	Date Plam	20' Trunk
Erythrina caffra	Kaffirboom Coral Tree	48" Box
Erythrina humeana	Natal Coral Tree	48 Box
Ficus microcarpa	Indian Laurel Fig	24" Box
Archonto phoenix "Cunninghamiana"	King Palm	20' Trunk

#### 2. Parcel Shrubs and Small Trees (Minimum 5 gallon size)

<u>Species</u>	<u>Common Name</u>
Magnolia soulangiana	Saucer Magnolia
Melaleuca nesophila	Pink Melaleuca
Comarostaphyus diversifolia	Summer Holly
Eriobotrya japonica	Loquat
Punica granatum	Pomegranate
Nerium oleander	Oleander
Rhapiolepis indica	Indian Hawthorn
Abelia grandiflora	Glossy Abelia
Xylosma congestum	Xylosma
Rhododendron Sp.	Azalea
Pittosporum tobira "carpa"	Wheeler's Dwarf - Tobira
Carissa macrocarpa	Natal Plum
Escallonia exoniensis	Dwarf Escallonia

#### 4. Parcel Ground Covers and Turf

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
Trachelosperum jasminoides	Star Jasmine	1 Gallon
Agapanthus africanus	Lilly of the Nile	1 Gallon
Hedera helix "Needle Point"	English Ivy	Flats
Liriope Muscari	Lilly Turf	1 Galon
Gazania Sp.	Gazania	Flats
Ophiopogon japonica	Mondo Grass	1 Gallon
Festuca elatior	Tall Fescue	Seed/sod
Cynodon Sp.	Bermuda Grass	Seed/sod
Stenotaphrum secundatum	St. Augustine Grass	Seed/sod



**LAX NORTHSIDE  
LANDSCAPE  
IMPLEMENTATION  
GUIDELINES**

<b>PROJECT AREA</b>	<b>RESPONSIBILITY</b>	<b>DOA When Westchester Parkway is constructed</b>	<b>Developer as part of site improvements</b>	<b>Developer of Lessor at time of Development</b>
<b>STREETSCAPE</b>				
Westchester Parkway La Tijera Boulevard Sepulveda Westway Emerson Avenue Lincoln Boulevard Loyola Boulevard Falmouth Avenue Pershing Drive		• • • • • • • •	• • • • • • • •	• • • • • • • •
<b>COMMON AREAS</b>				
Landscape Buffer			•	
Gateways				
Sepulveda Blvd. & Lincoln Blvd. La Tijera Blvd. Sepulveda estway Westchester Parkway and Sepulveda Westway Westchester Parkway and Pershing Drive		• • • • • • • •	• • • • • • • •	• • • • • • • •
Primary Intersections				
La Tijera Blvd. and Westchester Parkway - Trees - View Park - Special Paving & Finishes		• • • •	• • • •	• • • •
Westchester Parkway and Lincoln Boulevard Major Entries		• • • •	• • • •	• • • •
<b>PARCELS</b>				
Landscape Setbacks - Westchester Center - Business Park			• • •	• • •
Courtyards/Plazas/Interior Landscaping				• • •
Parking Lot Landscaping				• • •



# **7.0 LIGHTING GUIDELINES**

## 7.0 Lighting Guidelines

The positive night time image of *LAX Northside* is important because it conveys a safe, secure, well designed, and organized development area. Special lighting of areas such as key intersections, transit stops and public plazas will greatly enhance the aesthetic character of the development area. The use of special lighting will be accomplished without impacting the surrounding neighborhoods or airport operations.

### 7.1 Parcel Site Lighting

#### Objectives

- To have on site-lighting contribute to the safe and efficient use of a development site.
- To have on-site lighting contribute to site security.
- To have on-site lighting complement and reinforce the architectural and site design character.
- To have on-site parking lot lighting fixtures and illumination levels be consistent throughout *LAX Northside* development.
- To prevent on-site lighting from casting glare onto adjacent streets and property.
- To encourage lighting design that is in conformance with energy saving guidelines.

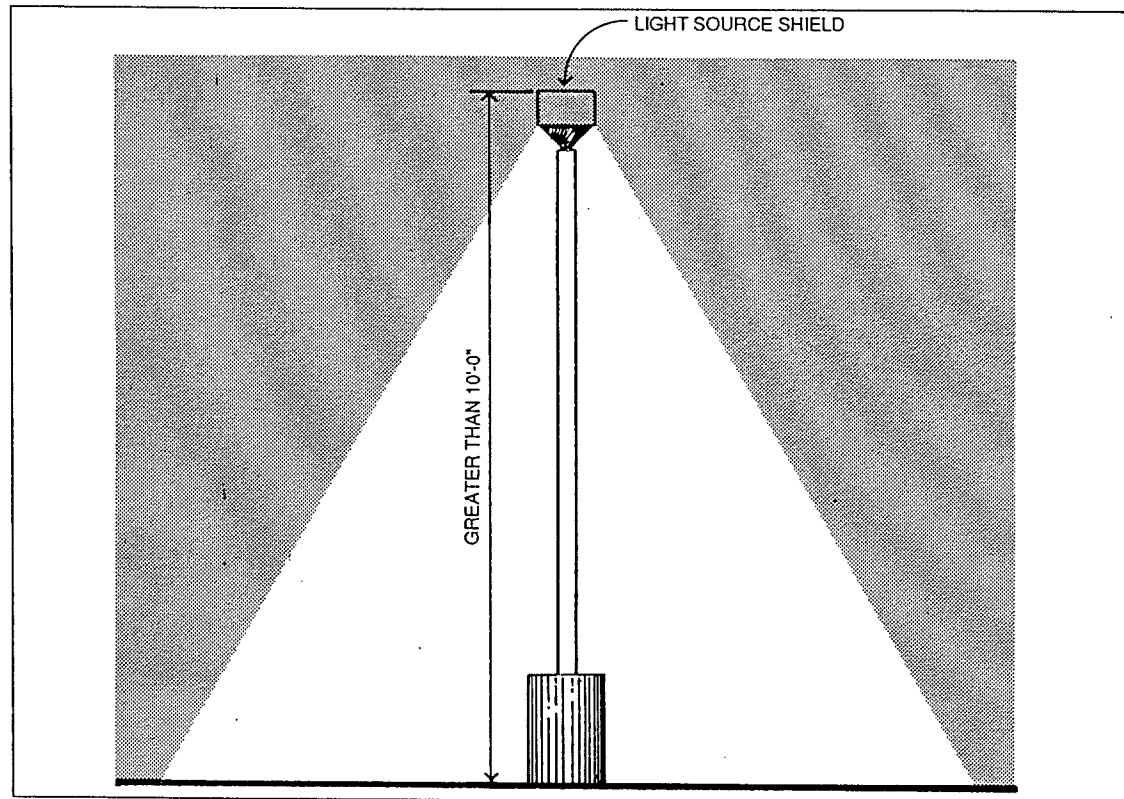
#### General Guidelines

##### 1. Cut-off Lighting

- a) All lighting potentially visible from an adjacent street except bollard or pole up to 10 feet in height shall be indirect or shall incorporate a full cut-off shield type fixture.

##### 2. Parking Lot

- a) Parking lot lighting shall include: parking areas, access drives, and internal vehicular circulation areas.



Cut-Off Shield Lighting

- b) As a minimum, site lighting on all lots shall provide side and rear property line pole lighting mounted on a three foot cylindrical concrete base with a fixture height sufficient to provide the minimum standard site lighting set forth herein. The concrete base finish shall be L.M. Scofield Co. T-21SD fractured finish grooved or equivalent.
- c) All such lighting fixtures shall be Kim SBC (Square Beam Cut-Off) or equivalent. The light source shall be a 150 watt color corrected high pressure sodium lamp.
- d) The parking lot illumination level shall achieve a uniformity ratio of 3 to 1 (average to minimum) with a maintained average of 1 foot candle and a minimum of .3 foot candle.

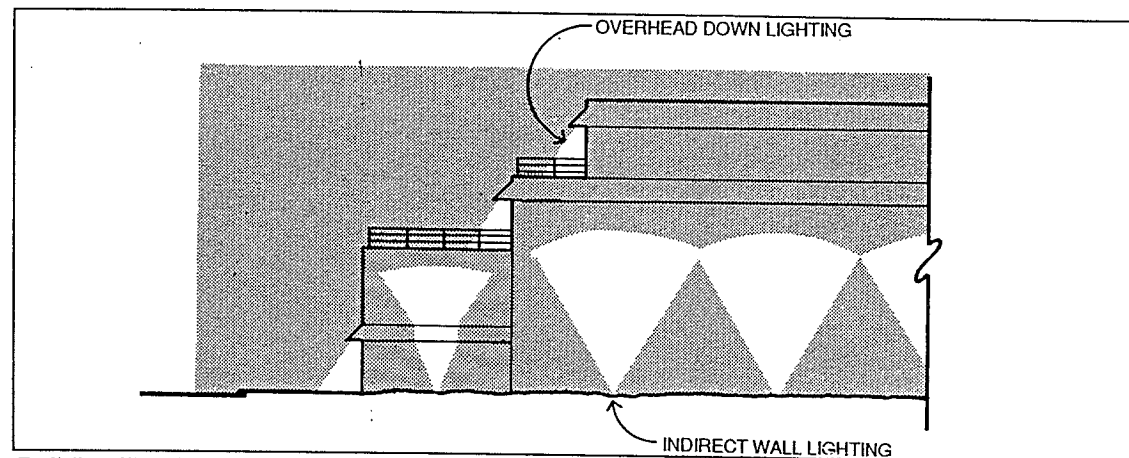
### 3. Service Area

- a) Service area lighting shall be contained within the service yard boundaries and enclosure walls. No light spillover should occur outside the service area. The light source should not be visible from the street.

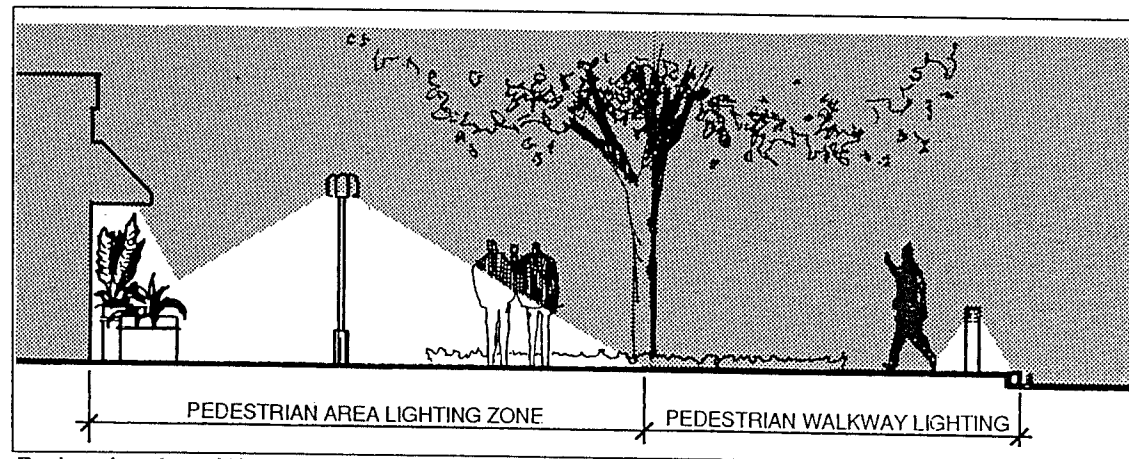
### 4. Building Illumination

- a) Building illumination and architectural lighting shall be indirect in character (no light source visible). Indirect wall lighting or "wall washing", overhead down lighting, or interior illumination

which spills outside is encouraged. Architectural lighting should articulate and animate the particular building design as well as provide the required functional lighting for safety and clarity of pedestrian movement.



*Building Illumination*



*Pedestrian Area Walkway Lighting*

5. Pedestrian Area Walkway

- a) Pedestrian walk and area lighting is segmented into two zones. The first zone is pedestrian area lighting. This includes outdoor pedestrian use areas such as courtyard, entryway, etc. Pedestrian area lighting should achieve a uniformity ratio of 3.5 to 1 average to minimum, with an average illumination of .60 foot candles and a minimum of .18 foot candles. The second zone is pedestrian walk lighting where point to point lighting is acceptable with no specific illumination levels required. The main emphasis in this zone should be to clearly identify the pedestrian walkway and direction of travel. These fixtures shall be of a consistent design within the specific project. Recommended fixtures for the two zones are Bega Series 9900 and 95903, respectively.

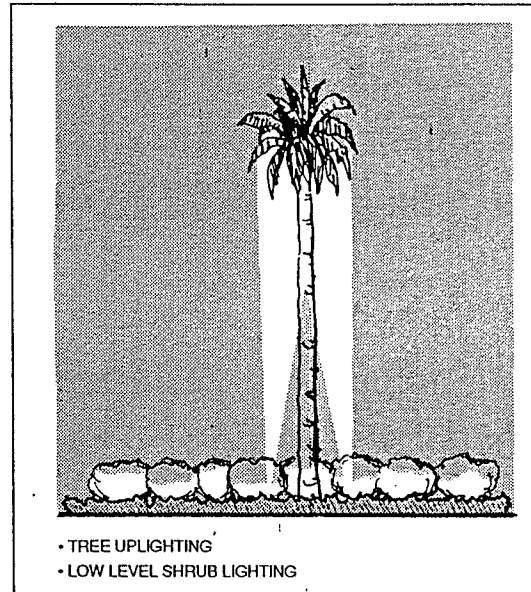
6. Landscape Accent Lighting

Landscape accent lighting can be provided at particular accent or focal point with up lighting of trees. All accent lighting shall be directed away from vehicular traffic.

## 7.2 Public Right of Way Lighting

### General Guidelines

All streets shall conform to the City of Los Angeles street lighting standards.



*Landscape Accent Lighting*

# **8.0 STREET FURNITURE**





## 8.0 Street Furniture

Street furniture elements include bench seating, bollards, planters, and trash receptacles located in public rights-of-way at locations such as transit stations, bus shelters, street intersections, and public plazas where high numbers of pedestrians commonly congregate. The street furniture shall be a unified system which reinforces a common design theme through standard color, materials, form, and high quality.

### Objectives

- To provide street furniture that is functional, durable, and easily maintained.
- To provide a coordinated and consistent high quality design theme which supports the visual image of *LAX Northside*.
- To implement street furniture which provides access and ease of use to handicapped persons.
- To provide amenities required to promote safe, visually pleasing, comfortable pedestrian environments.

### General Guidelines

#### 1. Bench Seating

- a. Benches shall be located adjacent to walkways, with a maximum distance of 40' between each seating area. In addition, bench seating shall be located in appropriate quantities to respond to users needs at transit stations, bus shelters, street intersections, and public plazas.
- b. All such benches shall be Ultrum metal series benches, or equivalent. Bench color shall be consistent with other street furniture.

#### 2. Bollards

- a. Bollards shall be located at street intersections where they will be used to define the boundary between pedestrian and vehicular zones. Bollards may also be used to delineate pedestrian walkways.
- b. All such bollards shall be Bega 95905 or equivalent with black finish and anchored with No. 896 system.

#### 3. Planters

- a. Planters may be used at public entrances to parcels or in conjunction

with other street furniture such as benches, bollards, or trash receptacles. Planters should be located in high amenity zones where large concentrations of pedestrians are likely to frequent.

- b. All such planters shall be Ultrum UL-3000 series consistent in color with benches.
- c. All planters shall have permanently installed irrigation systems. Plant material shall be replaced as necessary in order to maintain a high quality appearance.

#### 4. Trash Receptacles

- a. Trash receptacles shall be located along walkways, near parcel entrances, seating areas, transit stops, public plazas, and other pedestrian gathering areas.
- b. All such receptacles shall be ultrum UL-3000 series receptacles or equivalent, and consistent in color with benches.
- c. All receptacles shall be located for ease of pick-up/trash removal.



# **9.0 SIGNING AND GRAPHICS**



## 9.0 Signing and Graphics

The signing standards contained in these guidelines will be used to regulate all signs in the Project. Any signing in the Project must comply with these guidelines in order to receive approval by the Department of Airports, which must approve all signing within airport property. Details of the submittal process are outlined in Section 8.6. In addition to conforming with these guidelines, all signs must comply with any codes, regulations and/or ordinances already imposed on this area. Permits must also be obtained through the usual civic channels.

These guidelines have several basic purposes:

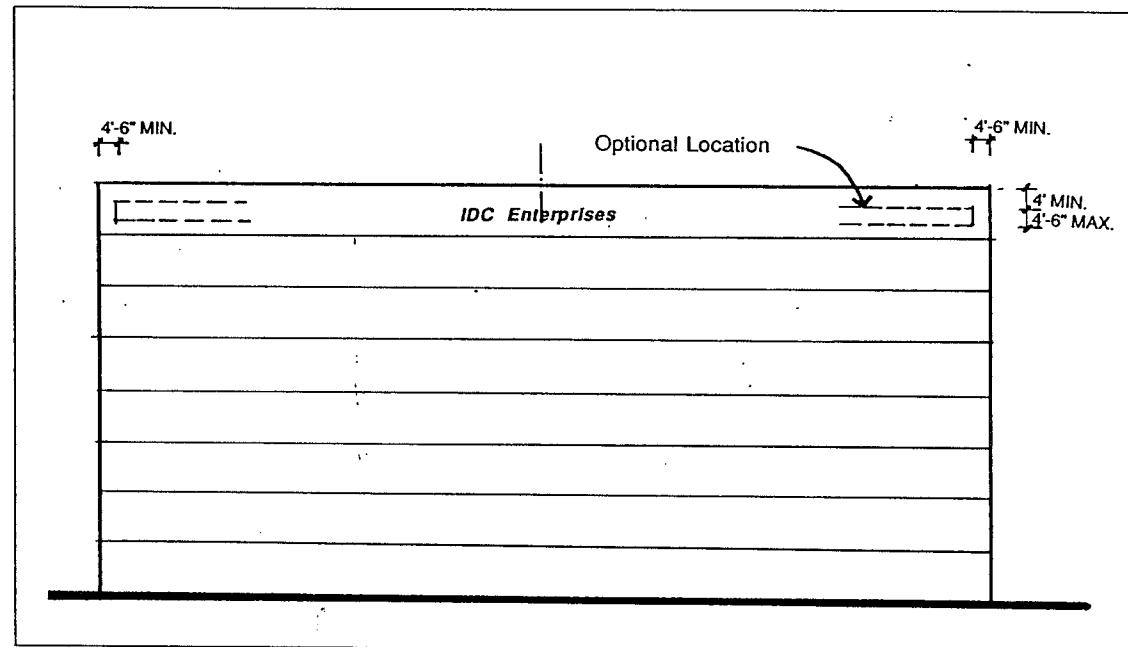
- To create an image and identity for the project through signing that is consistent, compatible and harmonious.
- To establish a high level of aesthetic design quality and a method of reviewing signs for conformance with that quality.
- To reinforce the park-like atmosphere of the project and create a feeling of community pride within the project.
- To create a positive contribution to the environment.
- To minimize delays in the design process
- To create a sense of order for all project signing elements.

These guidelines are divided into four sections based on the area of application or, in the case of retail guidelines, the type of business.

In addition to general guidelines, *Westchester Center* Guidelines and *Business Park* Guidelines this section also includes Retail Guidelines which apply to restaurants, hotels, markets, or any other project support businesses whom the DOA deems qualifies for these specific guidelines.

### 9.1 Tenant Identification Signs

Identification (I.D.) as is, refers to, and shall be limited to, tenant's trade name only. Tenant's customary signature or logo, hallmark, insignia, or other trade identification may be permitted at the DOA's sole discretion.



*Tenant Identification Sign Location*

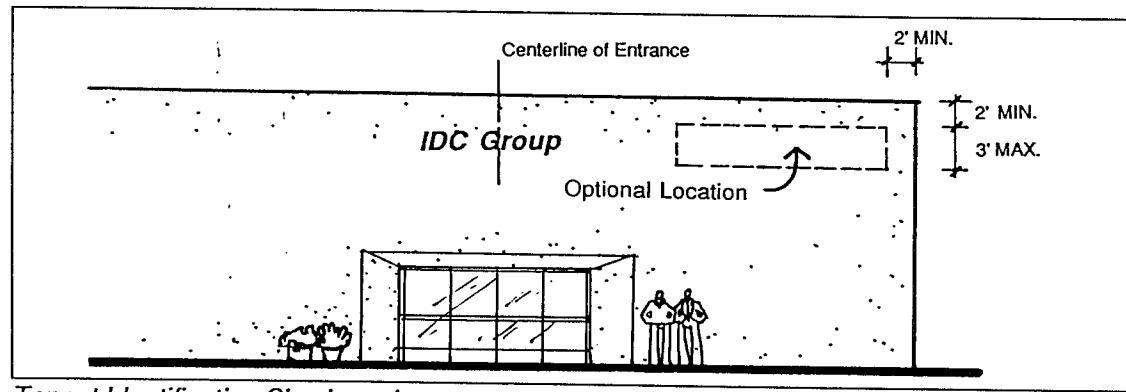
### General Guidelines

1. Tenant shall be permitted to install two (2) illuminated identification signs on two separate elevations of the building.
2. No signing shall be visible from the immediately adjacent Westchester residential areas.
3. Illumination brightness of signs is subject to approval by the DOA and must comply with all applicable building and electrical codes. Surface brightness of all translucent materials shall be consistent in all components of sign. Light leaks will not be permitted. All conduits, lamps or transformers shall be entirely concealed within the sign and not visible from the surface.
4. Tenant shall be responsible for the cost of signs fabricated and/or installed which do not conform to these criteria/guidelines, and the cost of their removal.
5. Suggested sign styles for tenant identification signs:
  - Individual, self-contained, internally illuminated letter forms.
  - Individual reverse channel, neon illuminated letterforms (halo effect).

6. The following are prohibited for all areas within *LAX Northside*.
  - Exposed light source (neon, incandescent).
  - Signs employing animated components, moving/flashing or blinking lights, exposed raceways, exposed ballast boxes or transformers, unedged or uncapped plastic letters or letters with no returns and exposed fastenings, luminous-vacuum formed type plastic letters, sandblasted wood type construction.
  - Sign manufacturer's names, stickers, stamps or decals.
  - Exposed fasteners.
  - Simulated materials (i.e., wood grained plastic laminate, etc.)

### Westchester Center Guidelines

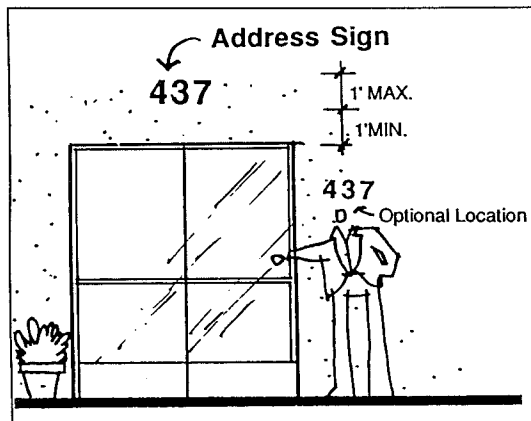
1. Each I.D. sign shall not exceed four feet six inches (4'- 6") in height, length to be determined by normal letterspacing of name, and must be installed within the parapet area of the facade, in accordance with the layout shown in signing Figure 1 (three possible locations shown)
2. Signs shall not overlap architectural features, such as mullions, window breaks, etc.
3. Signs will not be permitted to project above the building.
4. The maximum projection of the sign from the face of the building shall be twelve inches (12").



*Tenant Identification Sign Location*

### Business Park Guidelines

1. Each I.D. sign shall not exceed three feet (3') in height, length to be determined by normal letter-spacing of name, and must be installed within the parapet area of the facade either centered on the facade, or near the corner of the facade, as shown in Figure 2.
2. Signs shall not overlap any major architectural features, such as mullions, window breaks, etc.
3. Signs may not project above the building.
4. The maximum projection of the sign from the face of the building shall be eight inches (8").



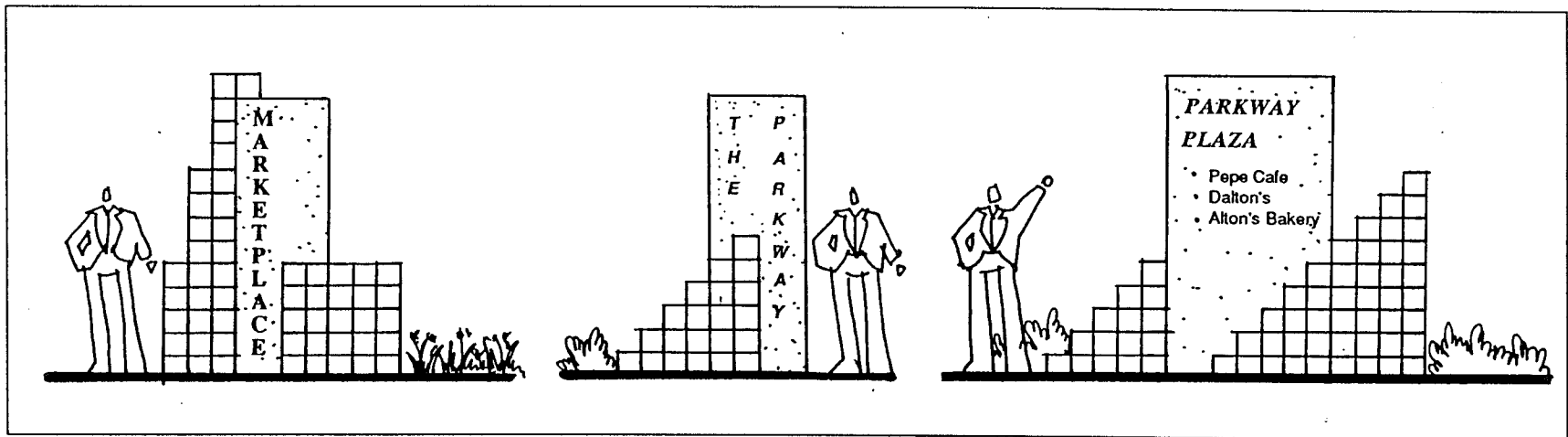
*Tenant Address Sign*

### Retail Guidelines

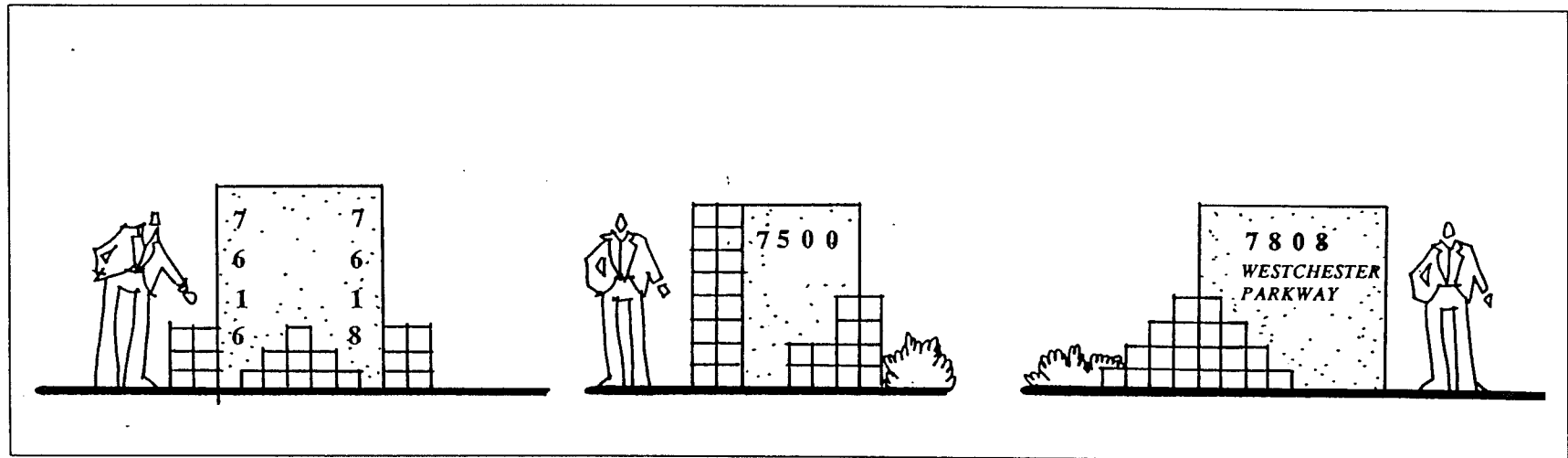
1. Tenant shall be permitted to install two (2) I.D. signs on facades of the building (maximum of 1 per facade can be illuminated), or if retail building is free standing, may choose to erect retail ground monument identification signs in any combination (maximum of 3).
2. If four or more retailers are attached together in a free standing building, they (together) shall be permitted to erect a project/complex ground monument sign.

## **9.2 Tenant Address Signs**

1. All tenants (regardless of area, including retail) will be required to install one (1) address sign (numbers only) not more than 1 foot in height, adjacent to main entrance.
2. Materials should be same as used in I.D. signs, and in same type style (font). Sign need not be illuminated, but should be highly visible for emergency vehicles as well as visitors.

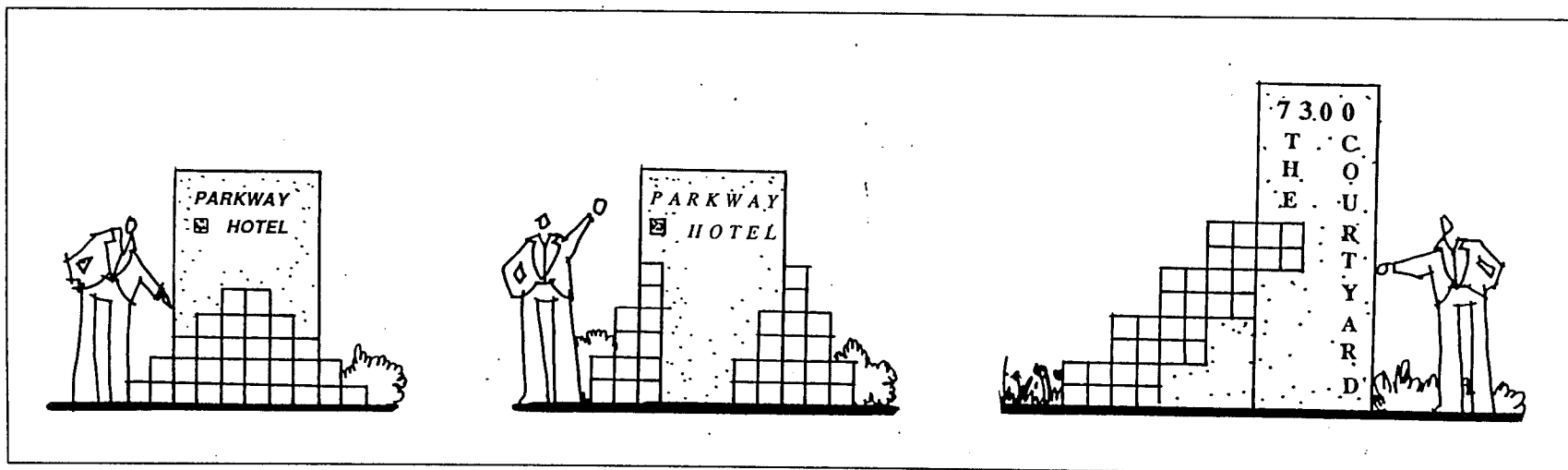


*Retail Ground Monument Signs*

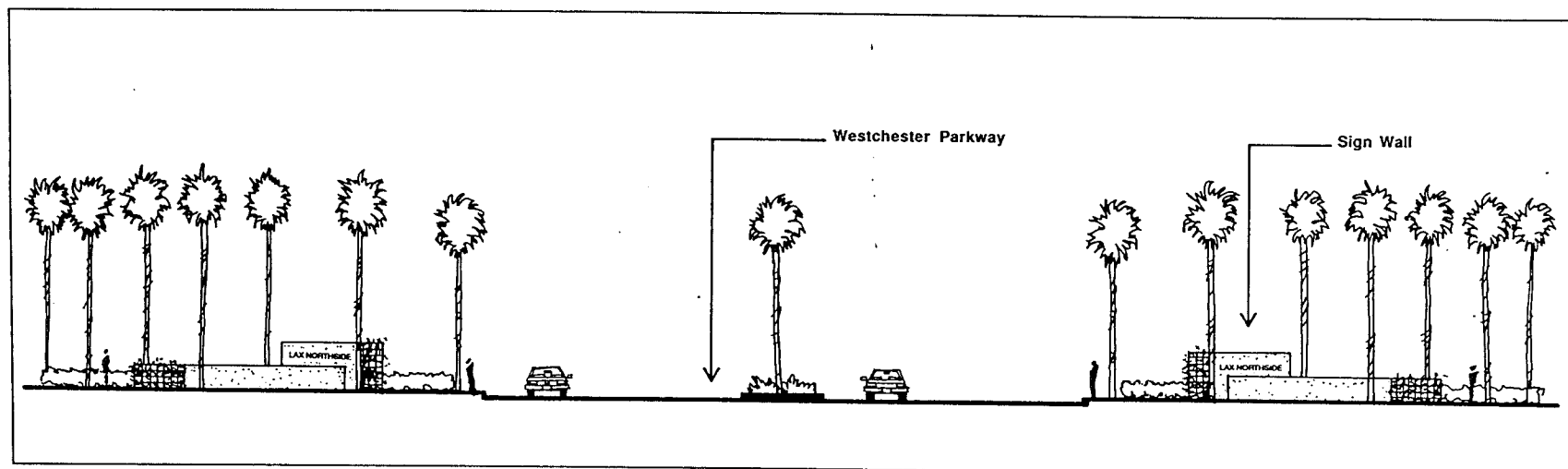


*Office Building Ground Monument Signs*





Hotel Ground Monument Signs



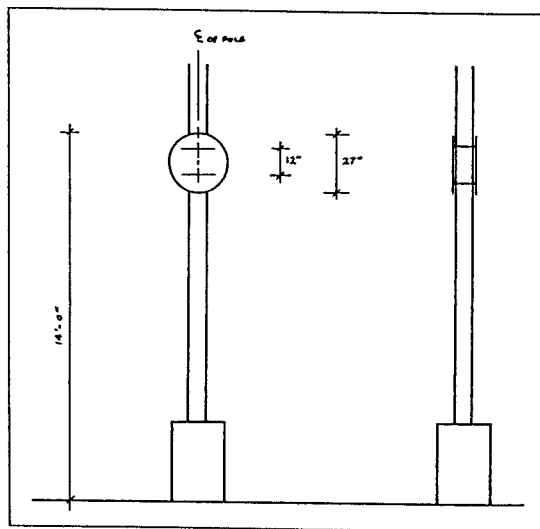
Entrance Sign Wall

## 9.3 Parking Signs

### General Guidelines

All tenants will have the same guidelines for parking signs.

1. Tenants shall provide handicapped parking in accordance with applicable City/County/State codes by surface painting the pictogram, (provided), in each required space, to the size and location indicated.



*Parking Area Designation Signs*

2. "No Parking" signs may be erected as necessary to prohibit parking in fire lanes only. Signs are to be City standards in size, verbiage, materials and construction, and shall be placed in intervals not less than forty (40) yards apart (same side of the street).
3. Area Designation Signs are to be used only in large surface lot area by permission of the DOA. Landlord will determine maximum quantity of signs permitted, according to size and configuration of lot. Signs shall conform in size and layout shown in Figure 8.
4. Tenants with parking structures will be required to install one (1) Parking Structure Exterior Entrance Sign (can be illuminated) indicating:
  - Who is allowed to park within (monthlies only/visitors)
  - Entrances and exits (no larger than opening)
  - Clearance information (immediately above entries)
  - Rates (if applicable)

Materials should be the same as used on I.D. signs.

## 9.4 Temporary Signing

### General Guidelines

1. The only temporary signing allowed within the project will be:
  - Construction walls/barricades
  - Construction signs
  - Lease signs
2. The overall project name, *LAX Northside* shall be given equal mention, each time the specific development name is mentioned.
3. Lease signs attached to buildings shall be limited to the ground floor.

## 9.5 Flags, Banners and Pennants

### General Guidelines

1. A maximum of three (3) flag poles and flags will be allowed for each building. Flag poles should be no taller than 30 feet. In no case, however, should flag poles exceed FAA height restrictions. Flags should be no larger than 6' x 9'. Banners and pennants shall only be permitted in interior court areas and not visible from building exterior, or by permission of the DOA only.
2. Special occasion banners or pennants may be temporarily erected only with the Landlord's (DOA) permission, obtained prior to installation.

### Submission (All Signs)

Tenant shall submit for approval three (3) complete sets of sign drawings to the DOA (Department of Airports), prior to sign fabrication and installation.

In addition to detailed sign drawings, such submission must include:

- A. Appropriate elevations of buildings showing design, location, size, message and layout of signs drawn to scale indicating dimensions, attachment devices and construction details.

- B. Sample board showing colors and materials of signs and adjacent surfaces.
- C. Section through signs showing projection from face of building, indicating dimensions.

Submission will be reviewed and either approved as submitted, approved with conditions or not approved.

## 9.6 Maintenance

### General Guidelines

1. Every sign shall be maintained in a clean, safe and good working condition, including the replacement of defective parts, defaced or broken faces, lighting and other acts required for maintenance of the sign. The display surfaces shall be kept neatly painted or finished at all times.
2. The base of any sign erected on the ground shall be kept clear of weeds, rubbish or other combustible material at all times.
3. Not more than ninety (90) days after the cessation of a business activity, service or product, the related signs shall be removed, or the face of said signs shall be removed and replaced with blank panels painted to match adjacent background colors.
4. If a tenant is negligent in maintaining or repairing or removing signing, the DOA will have necessary work performed at the lessee's expense.



# **10.0 DESIGN REVIEW PROCESS**



## 10.0 Design Review Process and Developer Participation

A design review process will assist developers in identifying issues and requirements ahead of time before costly investments in plans are made and will ensure that development at *LAX Northside* meets the requirements of these guidelines. This review process will satisfy the requirements of the Department of Airports only. Additional review by the FAA, the City of Los Angeles, Cultural Affairs Commission, and the City of Los Angeles Departments of Planning and Building and Safety will be required. The Department of Airports will assist the developer in scheduling these reviews.

The Department of Airports review will take place in four steps:

- Pre-design
- Conceptual Site Plan Review
- Design Development Review
- Construction Document Review

At each review step, the project will be reviewed and be approved as submitted, approved with conditions, or not accepted.

### Pre-Design

The developer will confer informally with Department of Airport staff to review the developer's proposed program and to identify po-

tential issues. At this meeting detailed requirement for future review will be established. A proposed program and generalized building location will be reviewed and approved for compatibility with adjacent development and conformance with these guidelines.

### Conceptual Site Plan Review

At this stage, a conceptual site plan will be submitted which shows the location, footprint, and floor area of all buildings; building heights, vehicular access and circulation, parking areas including provisions for van and carpool spaces, pedestrian circulation, landscape and hardscape elements, service areas, and any other information determined to be needed. Preliminary architectural concepts may be submitted at this time.

### Design Development Review

This review will evaluate a specific design for compliance with the design guidelines, zoning conditions, and any conditions imposed during the first two reviews. These plans should be a development of the already approved site plan.

Drawings submitted at this stage should include a site plan, architectural, landscape, signing and lighting plans sufficient to illustrate design intent and the methods which are being used to satisfy the requirement of these guidelines.

### Construction Documents

All construction documents will be reviewed to assure that they conform with the approved design plan and that any conditions of approval have been satisfied.

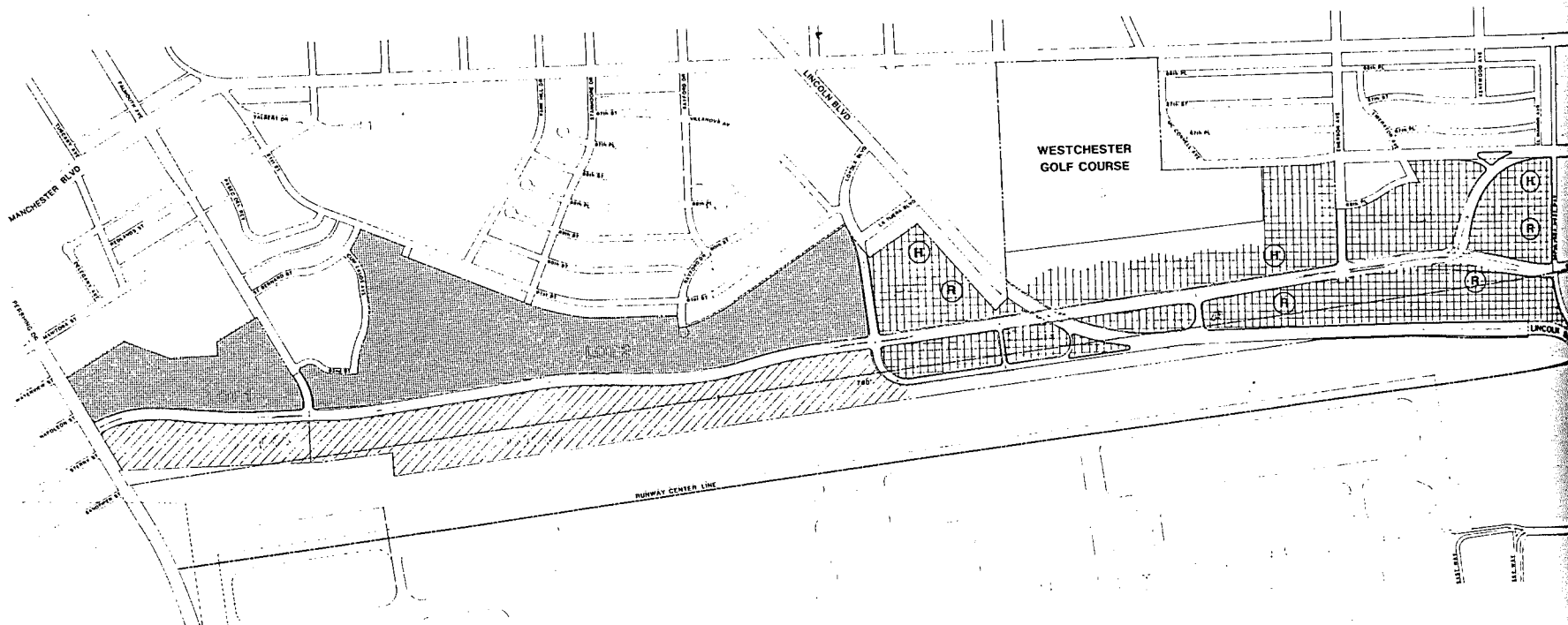
### Compliance Verification

Construction will be monitored to verify compliance with the Design Guidelines and the approved construction drawings.

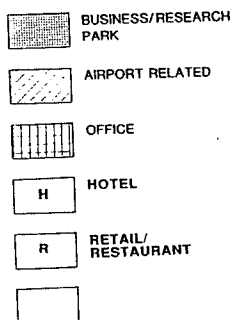




# **11.0 ZONE CHANGE CONDITIONS**



## LEGEND



### "Q" QUALIFICATIONS

- PARCEL 1 OFFICES, BUSINESS PARK AND RESEARCH DEVELOPMENT CENTER.
- PARCEL 2 OFFICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER.
- PARCEL 3 COMMERCIAL USES, INCLUDING HOTEL, OFFICES, RESTAURANTS, SERVICE AND RETAIL USES.
- PARCEL 4A LIGHT INDUSTRIAL USES, AIRLINE AND AIRPORT SUPPORT SERVICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER; AND OFFICES WITHIN 400 FEET OF PERSHING DRIVE.

- PARCEL 4B LIGHT INDUSTRIAL USES, AIRLINE AND AIRPORT SERVICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER; AND OFFICES WITHIN 400 FEET WEST OF LOYOLA BOULEVARD.
- PARCELS 5, 6 and 7 OFFICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER; AIRLINE AND AIRPORT SUPPORT AND ACCESSORY USES; AIRPORT VIEW SITE.
- PARCEL 8 COMMERCIAL USES, INCLUDING OFFICE AND RESTAURANT USES; AND AUTOMOBILE SERVICE STATION.

- PARCEL 9 COMMERCIAL USES, INCLUDING OFFICE AND RESTAURANT USES; AUTOMOBILE SERVICE STATION IN THE WESTERN PORTION ONLY; PUBLIC AUTOMOBILE PARKING IN THE EASTERN PORTION ONLY; AIRPORT VIEW SITE.
- PARCEL 10 PUBLIC AUTOMOBILE PARKING.
- PARCEL 11 COMMERCIAL USES, INCLUDING HOTEL, OFFICE, RESTAURANT, SERVICE AND RETAIL USES, AND A MOVIE THEATER COMPLEX.
- PARCEL 12A COMMERCIAL USES, INCLUDING OFFICES, HOTEL, RESTAURANT, SERVICE AND RETAIL USES.
- PARCEL 12B COMMERCIAL GOLF COURSE.

Permitted Uses

PARCELS. UV W X Y Z

## 11.0 Zone Change Conditions

### (Q) Qualifications - Conditions of Approval

1. The subject property shall be used for those principal and accessory uses in development areas as indicated on the attached map. (Exhibit E-1), as follows:

#### a. Area East of Lincoln Boulevard

Parcel 8 - Commercial uses, including office and restaurant uses; and automobile service station.

Parcel 9 - Commercial uses, including office and restaurant uses; automobile service station in the western portion only; public automobile parking in the eastern portion only; and an airport view site.

Parcel 10 - Public automobile parking.

Parcel 11 - Commercial uses, including hotel, office, restaurant, service and retail uses, and a movie theater complex.

Parcel 12A - Commercial uses, including offices, hotel, restaurant, service and retail uses.

Parcel 12B - A commercial golf course, including golf driving tees and ranges and similar commercial golf uses.

#### b. Area Between Lincoln Boulevard and Falmouth Avenue (Extended)

Parcel 2 - Offices, business park and research and development center.

Parcel 3 - Commercial uses, including hotel, offices, restaurants, service and retail uses.

Parcel 4B - Light industrial uses, airline and airport support services, including flight kitchens, caterers, ticket reservation centers, custom house brokers, warehouse, airport maintenance and ground services (excluding engine shops), freight forwarders, package delivery services, security services, import/export services and other similar accessory or support services; business park and research and development center; and offices within 400 feet west of the prolongation of Loyola Boulevard on the south side of Westchester Parkway.

Parcels 5, 6, and 7 - Offices, business park and research and development center; airport view site; airline and airport support and accessory uses.

#### c. Area West of Falmouth Avenue (Extended)

Parcel 1 - Offices, business park and research and development center.

Parcel 4A - Light industrial uses, airline and airport support services, including flight

kitchens, caterers, ticket reservation centers, custom house brokers, warehouse, airport maintenance and ground services (excluding engine shops), freight forwarders, package delivery services, security services, import/export services and other similar accessory or support services; business park and research and development center; and offices within 400 feet east of Pershing Drive on the south side of Westchester Parkway.

2. No aircraft or engine run-ups shall be permitted within the project boundaries.

3. The height of structures shall be controlled as follows:

- a. Parcel 2 - No structure located within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. All accessory facilities on the roofs of such buildings, such as air conditioning units and other equipment, shall not be visible from nearby residential properties to the north and are exempted from the height restriction.

- b. Parcels 1 and 4A - No structure located within 200 feet of the north airport property

line between Falmouth Avenue and Pershing Drive shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. No structure located within Areas 1 or 4A south thereof shall exceed four stories (including parking levels) or 55 feet in height measured from the finished grade of the lot. All accessory facilities on the roofs of buildings within Parcel 1, such as air conditioning units and other equipment, shall not be visible from nearby residential properties to the north and are exempted from the height restriction.

4. Structures shall observe minimum setbacks, as follows:
  - a. Parcel 2 - All structures shall observe a minimum setback of 50 feet from the north airport property line between Loyola Boulevard and Hastings Avenue. Open parking areas are permitted within the required building setback area, except as may be modified by required landscape buffer setbacks.
  - b. Parcel 1 - All structures shall observe a minimum setback of 100 feet from the north airport property line between Falmouth Avenue and Pershing Drive. Open parking areas are permitted within the required building setback area, but not within the required landscape buffer setback.

5. Landscaped buffer setbacks shall be required, as follows:

- a. Parcels 11 and 12A - A 30-foot-wide landscaped buffer setback along 88th Street between Sepulveda Westway and Liberator Avenue shall be required containing no buildings or structures, except for walls or fences, and include trees, 15 gallons and ten feet tall at the time of planting, planted at a maximum of 30 feet apart, of a type similar to those contained in Section 7 of the Planning Department's Technical Report of "Shrubs and Trees for Landscaping and Screening". Further, a 15-foot-wide landscaped buffer setback along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site) shall be required and include a dense screen of closely planted evergreen trees.
- b. Parcel 2 - A 15-foot-wide landscaped buffer setback along Cum Laude Avenue and 92nd Street (adjacent to the St. Bernard High School site) shall be required and include a dense screen of closely planted evergreen trees.
- c. Parcel 1 - A 50-foot-wide landscaped buffer setback along the north airport property line between Falmouth Avenue and Pershing Drive shall be required and include low, minimum-view-obstructing

shrubs and ground cover (no trees) which are compatible, where appropriate, with adjacent off-airport landscaping.

6. The total development of the subject property shall not generate more than 6,340 project-related inbound daily trips in the A.M. peak-hour, no more than 7,000 project-related outbound daily trips in the P.M. peak-hour (source: "Los Angeles International Airport Final Environmental Impact Report, LAX North Side Development Project, April 1983"). This determination shall be based on the highest average hourly A.M. and P.M. counts taken over a typical Monday through Friday period at exclusive project access drives and/or roadways. Such counts shall be taken at the expense of the Department of Airports of individual developers at intervals determined to be reasonable by the City Planning Department after consultation with the Department of Transportation; but in no event shall the first count be required before certificates of occupancy have been issued for buildings having a combined floor area in excess of 3,500,000 square feet, as defined in Section 12.21.1 (A,5 and B,4) of the Municipal Code.
7. Prior to the issuance of building permits, detailed development plans, including a complete landscape plan, shall be submitted to the Department of City Planning for approval.

8. Prior to the issuance of building permits, an on-site roadway/highway construction phasing plan and individual parcel access plan shall be submitted to the Department of City Planning for approval, and for coordination and review with the Department of Transportation and the Bureau of Engineering.

9. Prior to the recordation of a final tract map covering all or any portion of the subject property, and prior to the approval of any variance granting use of any portion of the property before recordation of a final tract map, the Board of Airport commissioners shall cause to be prepared and shall approve a project design plan, together with developmental guidelines for the entire project area. The primary purposes of the design plan and guidelines shall be to provide future developers with a unifying theme and design handbook and to provide the Department of Airports with a basis for reviewing and coordinating project development plans. The project design plan shall establish a unifying architectural theme, while the elements of the developmental guidelines shall include consideration of: Building design, height, bulk, locations and interrelationships; building materials, colors and textures; public and private area landscaping; public and private signage and graphics; airport view sites; street furniture; exterior

lighting; internal pedestrian and vehicular circulation; parking area design; on- and off-airport interfaces; and other design considerations as may be appropriate. The project design plan and developmental guidelines shall be governed by the conditions of approval attached to this change of zone, wherever relevant.

10. The project design plan and developmental guidelines shall treat Parcel 11 as a single comprehensive planned unit, providing a compatible interface with the existing and potential future uses on the privately owned property adjoining the east side of Sepulveda Westway between La Tijera Boulevard and Will Rogers Street. Convenient pedestrian access to Parcel 11 shall be provided from Sepulveda Westway, and the Sepulveda Westway frontage shall be attractively landscaped. The design of Parcel 11 shall provide, to the extent practicable and feasible, for visual continuity and accessibility between Parcel 11 and the properties on the east side of Sepulveda Westway:

11. All open areas not used for buildings, driveways, parking areas, recreational facilities, or walks shall be attractively landscaped in accordance with a landscape development plan prepared by a licensed landscape architect or licensed architect. Approved copies of such plans shall be submitted to the Department of

Building and Safety before issuance of a building permit. All landscaped areas shall be equipped with automatic sprinklers and shall be maintained in a first-class condition at all times. All types of plants selected and required watering system for such landscaping shall, to the extent possible, conserve water and shall be consistent with any water conservation ordinance enacted by the City.

12. All open parking areas located within Parcel 1 shall devote at least 4 percent of the parking area to landscaping, with at least three-fourths of the 4 percent devoted to interior landscaping distributed throughout the parking area.

13. A 6-foot high chain link fence shall be provided along the airport property line in the following locations: Along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site in Parcel 12A); along Cum Laude Avenue and 92nd Street (adjacent to the St. Bernard Senior High School site in Parcel 2); and between Falmouth Avenue and Pershing Drive (along the north boundary of Parcel 1).

14. A solid 8-foot high masonry block wall shall be constructed along the north airport property line between Loyola Boulevard and Hastings Avenue (along the north

boundary of Parcel 2) and trees and tall shrubs shall be planted immediately south of the wall on airport property to provide additional visual screening above the wall. The wall shall be installed and measured in height from the finished grade of the alley between Loyola Boulevard and Rayford Drive, and from the finished grade of the adjoining residential lots between Rayford Drive and Hastings Avenue.

15. All development on the property shall provide off-street parking on the following bases: One space for each guest room; One space for each guest room; one space for each 300 square feet of floor area for office uses and for business parks and research and development centers; one space for each 250 square feet of floor area for retail and service commercial uses; 16 spaces for each 1,000 square feet of floor area for restaurants; and not less than three stalls for each four employees on the main shift for light industrial uses. Floor area shall mean the area enclosed within the walls of buildings, exclusive of floor area devoted to off-street parking or accessory areas, as defined under Section 12.21-A, 4 of the Municipal Code. The Director of Planning may modify these parking standards where appropriate to permit reduced parking in mixed-use or joint-use facilities.

16. The hours of operation for trash pick-up or freight deliveries or pick-up, within 300 feet of the north boundary line of the project, shall be limited to between 7 a.m. and 9 p.m.

17. Adequate protection against exterior noise shall be included in the design and construction of hotels and motels. Adequate protection shall mean a noise reduction (exterior to interior) sufficient to insure that the interior community noise equivalent level (CNEL) in all habitable rooms does not exceed 45db during aircraft operations.

18. All central air heating and/or air conditioning units shall be installed with an air filtration system (either charcoal or electronic) to improve the air quality effects on the project occupants. This requirement shall not preclude the installation of operable windows for passive or natural heating or cooling opportunities.

19. The use of any outdoor public address or paging system shall be permitted only south of the Westchester Parkway and shall utilize: (1) a low-pressure speaker system with each speaker having an audible range limited to a 400-square-foot area and placed a maximum of 40 feet apart, or (2) a sound system designed by a qualified sound engineer so as to reduce the impulse noise level to inaudibility

beyond the premises of the establishment.

20. In all industrially zoned areas, all buildings, and enclosing walls or fences shall be constructed, the machinery and equipment shall be so installed and maintained and the activities shall be so conducted that all noise, vibration, dust, odor and other objectionable factors shall be confined or reduced to the extent that no reduction in the use of property will result to persons residing adjacent to the subject property. Whenever there is any difficulty in determining the application of these provisions to any specific case, the Department of Building and Safety shall make such determination.

21. All building signs shall be designed by the architect of the building or facility. These building identification signs shall be wall signs designed for placement on the face of the proposed building and not projecting above the roof or parapet wall. Except that, free-standing identification or project directory signs not exceeding 50 square feet in area on each face and not exceeding 8 feet in height may be permitted, subject to individual review and approval by the Department of Airports.

22. All lighting shall be directed onto the site and no flood-lighting shall be located as to

be seen directly by the adjacent residential areas. This condition shall not preclude the installation of low-level security lighting.

23. All buildings within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall be designed so that all second-story or higher windows that overlook the adjacent single-family residential homes shall not be transparent below an eye level of 6 feet on any floor, but shall be translucent; or the building shall be designed in such a way that windows will not overlook the single-family residential properties to the north.

24. Vehicular driveway ingress and egress from the subject property shall be prohibited from Cum Laude Avenue and from 92nd Street (Parcel 2); from Lincoln Boulevard between Sepulveda Boulevard and the airport property line north of the Westchester Parkway (Parcel 6, 7, 8, 9, 10 and 12A); and from 88th Street, Liberator Avenue, 88th Place and Emerson Avenue (Parcel 12A).

25. In all industrially zoned areas, except Parcels 4A and 4B, all activities shall be conducted within an enclosed building. Within Parcels 4A and 4B, all outdoor activities shall be located on the southerly side of the buildings and screened from public streets.

26. All utilities within the project shall be installed underground.

27. All development within the project shall contain trash compactors to reduce the volume of solid waste to be removed from the site.

28. The Department of Airports and individual developers shall develop and implement a transportation system and parking management plan, which shall include the following as a minimum.

a. Individual developers shall encourage public transit use by offering subsidized transit passes to employees.

b. The Department of Airports shall evaluate the potential for subscription bus service for large concentrations of employees.

c. Each developer employing 500 or more permanent employees shall provide company-owned vans and/or offer subsidies to individual vanpool operators to encourage the formation of vanpools.

d. The Department of Airports shall make carpooling information available to building occupants and encourage formation of carpools and vanpools and shall establish a carpool-matching service.

e. Individual developers shall develop and implement programs to offer preferential parking to carpooling and vanpooling building occupants.

f. The Department of Airports shall make transit schedules, maps and other transit information available to building employees and users.

g. To the extent feasible, individual developers shall encourage flexible working hours.

h. The Department of Airports and each developer employing 500 or more permanent employees shall appoint a rideshare coordinator to administer the functions required by this condition.

29. Prior to major grading of the project site, a rodent control effort shall be undertaken to reduce the existing rodent population within the project area.

30. Developers of individual projects shall take the following steps relative to energy conservation:

a. Consult with the Los Angeles Department of Water and Power and Southern California Gas Company to determine feasible energy conservation features which could be incorporated into the design of structures, beyond those required by State and City laws.

b. Utilize, where cost-effective, alternative sources of energy such as solar water and pool heating.

c. Utilize the Department of Water and Power's energy pre-audits on all new industrial developments.

31. Individual developers shall consult with the Los Angeles Police Department on the provision of security measures for developments, which measures shall include the following at the minimum:

a. For each developer responsible for 200,000 or more square feet of total floor area (Los Angeles Municipal Code 12.21.1), guards shall patrol all levels of buildings and access to and from buildings shall be monitored by a guard on duty during nighttime and weekends.

b. Subterranean parking levels shall be lighted 24 hours a day.

c. All parking levels, corridors and arcades shall be lighted during the nighttime and weekends.

32. A qualified archaeologist shall be available, as needed, during site grading and subsurface operations, with the authority to assure reasonable measures to insure

protection and recovery of significant archaeological resources.

33. All conditions imposed by this action, with the exception of Condition No. 9 relating to a project design plan and development guidelines, may be fulfilled incrementally as individual phases of the subject property are developed.



In accordance with provisions of Section 17.03 of the Los Angeles Municipal Code, the Advisory Agency approved Tentative Tract No. 34386 composed of 12 lots, located on the north side of the Los Angeles International Airport (LAX) extending generally between Sepulveda Boulevard and Pershing Drive, with varying depths, for a proposed development of approximately 358 acres of land with commercial, recreational and airport-related industrial land uses, subject to the following conditions:

1. That Lincoln Boulevard between Sepulveda Boulevard and proposed Westchester Parkway be dedicated to a width of 120 feet as shown on the tentative map, together with suitable transitions to the existing dedications at each end and on- and off- ramps to Westchester Parkway satisfactory to the City Engineer and the Department of Transportation.
2. That Westchester Parkway between Sepulveda Westway and Lincoln Boulevard be dedicated to a width of 120 feet and that Westchester Parkway between Lincoln Boulevard and Pershing Drive be dedicated to a width of 110 feet, together with on- and off- ramps for the overpass at Lincoln Boulevard to the satisfaction of the City Engineer and the Department of Transportation.
3. That the extension of La Tijera Boulevard west of Sepulveda Westway to Westchester Parkway be dedicated to a width of 100 feet substantially as shown on the tentative map.
4. That a variable width strip of land be dedicated along the west side of Sepulveda Boulevard between Lincoln Boulevard and 96th Street to complete a 74.5-foot half-street dedication (Reference Plan No. P-26953).
5. That a minimum turning area be dedicated at the westerly terminus of 88th Street westerly of McLean Avenue is satisfactory to the City Engineer.
6. That a variable-width strip of land be dedicated along the south side of 88th Street at its intersection with proposed La Tijera Boulevard to provide for the channelization in accordance with Figure 4.7a, Page IV-59, of the Final EIR and as shown on the tentative map.
7. That an 18 foot wide strip of land be dedicated along the west side of Sepulveda Westway between La Tijera Boulevard and Lincoln Boulevard to complete a 43-foot half-street, together with a realignment at the intersection with Lincoln Boulevard.
8. That the intersection of Sepulveda Westway, 92nd Street and Lincoln Boulevard be realigned to improve the angle, location and traffic movements satisfactory to the City Engineer and the Department of Transportation.
9. That three foot wide strips of land be dedicated along the east and west sides of Emerson Avenue between Westchester Parkway and 88th Place, in accordance with secondary highway standards, together with a suitable turning area at its terminus south of 88th Place.
10. That the realignment of Loyola Boulevard be dedicated 86 feet wide and its easterly extension to Lincoln Boulevard south of Westchester Parkway be dedicated 64 feet wide, substantially as shown on the tentative map.
11. That Falmouth Avenue adjoining the tract be dedicated in accordance with secondary highway standards as shown on the tentative map unless it is deleted as a secondary highway prior to recordation.
12. That 8 foot, 40 foot and variable width strips of land be dedicated along the east side of Pershing Drive adjoining the tract to complete a 92 to 100 foot street dedication satisfactory to the City Engineer.

13. That for all street intersections in or adjoining the tract, the radius for property line returns shall be 20 feet for major and secondary highways, including local streets adjoining "M" zoned land and 15 feet for all other streets.
14. That arrangements be made with the Los Angeles County Flood Control District prior to recordation of the final map for any necessary easements and permits with respect to existing and/or new facilities.
15. That a covenant and agreement be recorded stipulating that the "airport drainage ditch," as shown on the tentative map, shall be maintained by the Department of Airports.
16. That prior to the issuance of a building permit for each development, two copies of a parking area and driveway plan be submitted to the appropriate district office of the Bureau of Engineering, and two copies be submitted to the Department of Transportation for approval, or that a covenant and agreement be recorded agreeing to do the same.
17. That the streets and alleys within the boundary of the tentative map, not shown as being retained, be permitted to be merged with the remainder of the subdivision pursuant to Section 66499.20-1/2 of the State Government Code, and in addition, the following be done and be administered by the City Engineer:
  - a. That consents to the streets and alleys being merged and waivers of any damages that may accrue as a result of such merger, be obtained from all property owners who might have certain rights in the areas being merged.
  - b. That satisfactory arrangements be made with all public utility agencies maintaining existing facilities within the area being merged.
18. That the Board of Airport Commissioners, by resolution, shall guarantee compliance with tract conditions in a manner satisfactory to the Advisory Agency and the City Engineer in lieu of posting bonds and recording covenant and agreements.
19. That a covenant and agreement be recorded stipulating that the Department of Airports will provide maintenance for the landscaped medians in Lincoln Boulevard, Westchester Parkway and Emerson Avenue, as required herein.
20. That the affected lots of the tract be restricted by the final map against vehicular access to and from:
  - a. Cum Laude Avenue, 92nd Street, St. Bernard Street and 91st Street.
  - b. Lincoln Boulevard between Sepulveda Boulevard and the airport property line northwesterly of Westchester Parkway.
  - c. Eighty-Eighth Street, Liberator Avenue, 88th Place, Emerson Avenue between 88th Street and 88th Place, and the alley southeasterly of 91st Street between Loyola Boulevard and Rayford Drive.
  - d. Rayford Drive, Stanmoor Drive, Hastings Avenue and Pershing Drive.
21. That the following requirements in connection with grading and construction in and adjacent to public right-of-ways be complied with in a manner satisfactory to the City Engineer:
  - a. Cut and fill slopes shall be no steeper than 2:1, steeper slopes may be allowed if calculations are submitted justifying these slopes. In no case shall slopes exceed 1-1/2:1.
  - b. The toes and crests of all slopes shall be located on private property and shall be set back 2 and 3 feet, respectively, from the property line.

- c. Where fill overlies a cut slope, the fill shall be keyed horizontally into bedrock a minimum width of 12 feet or the slope shall be over-excavated a minimum of one equipment width or 12 feet and replaced as a compacted fill slope.
  - d. All streets shall be founded upon firm, natural materials or properly compacted fill. Any existing loose fill, loose colluvial or alluvial soils, organics or landslide material shall be removed prior to placement of engineered fill.
  - e. Fill material shall be compacted to a minimum of 90 percent relative compaction, as defined in Bureau of Engineering Standard Plan S-610. Fill shall be benched into competent material.
  - f. Slopes that are not immediately proposed to be planted or sprinkled before the October-March winter rain season shall be sprayed with a non-toxic, non-flammable, non-polluting material such as/or equivalent to SOILBOND HP 401.
  - g. All slopes shall be planted and a sprinkling system installed as soon as possible after grading to alleviate erosion.
  - h. Slopes which daylight adversely dipping bedding shall be supported by either a retaining wall or designed buttress fill.
  - i. A final as-graded geologic and soils report shall be submitted to the City Engineer's Office prior to acceptance of City streets.
  - j. Prior to issuance of grading permits, a geologic and soils report covering the possibility of mud flows must be submitted to this office.
22. Prior to the recordation of the final map, the subdivider will prepare and execute four copies of a covenant and agreement (General Form) in a manner satisfactory to the Department of Building and Safety and the Planning Department, binding the subdivider and all successors to the following:
- a. Construct all exterior walls with double-pane glass and construct exterior walls and floor-ceiling assemblies in a manner to provide an airborne sound insulation system achieving an Ldn of 75, as defined by the Wyle Research Report WCR 74.3, June 1974, prepared for the City of Los Angeles (this condition applies to building areas used for offices, retail sales and restaurants).
  - b. That a solar access report shall be submitted to the satisfaction of the Advisory Agency prior to obtaining a building permit.
- c. That a summary of the solar report will be provided to lessees of the proposed subdivision.
  - d. That all construction activities be limited to weekdays during daylight hours.
  - e. That the lease agreements shall encourage that lessees develop active recycling programs to reduce the volumes of solid waste.
23. That the subdivider file a covenant and agreement satisfactory to the City Planning Department to the effect that the tract area will never be developed for residential use unless the required dedication of land or payment of fees in lieu thereof for park and recreational purposes have been made. Four copies shall be filed for approval by the Planning Department prior to recordation of the final map.
24. Prior to clearing any conditions and thus recordation, a copy of the final zone change ordinance to (T) (Q) C2-1 and (T) (Q) M2-1 Zones shall be submitted to the Advisory Agency. If there are any differences in the adopted ordinance and the Advisory Agency or City Planning Commission action, a modification will be required. The applicant will be required to furnish copies of the final zone change ordinance and Council directions for review and comparison.

25. The Fire Department requires the submission and approval of a plot plan prior to the recordation of the final map or the recordation of an agreement satisfactory to the Fire Department to the effect that the said plan will be submitted prior to issuance of building permits for the tract. Forms may be obtained at the Bureau of Engineering counter.

All access roads to be paved to City Engineer's requirements with a minimum width of 28 feet, or to the satisfaction of the Fire Department.

A Fire Department permit is required on all private fire hydrant systems.

The making of financial arrangements with the Department of Water and Power will indicate concurrence with the installation location of public fire hydrants.

All hydrant installations and enlargements to be completed prior to any street paving required for this project.

26. That the tract be permitted to record with final map units in a number and sequence satisfactory to the Advisory Agency in consultation with the Department of Transportation, City Engineer and Council office to assure adequate infrastructure in accordance with the level of development.

27. That prior to recordation of any final map units, the subdivider shall specifically request the City Planning Department through its preparation of the Los Angeles International Airport Plan and the Westchester-Playa Del Rey District Plan to initiate proceedings for the following:

- a. Delete the extension of La Tijera Boulevard westerly through the Westchester Golf Course to its connection to the new Westchester Parkway west of Lincoln Boulevard.
- b. The downgrading of 88th Street to a collector street between the proposed La Tijera connector and Emerson Avenue and to a local street west of Emerson Avenue.
- c. The deletion of Stanmoor Drive between the north project boundary and the new Westchester Parkway.
- d. The addition of the La Tijera Connector between 88th Street and the new Westchester Parkway as a major highway.
- e. The upgrading of the new Westchester Parkway from Sepulveda Westway to Pershing Drive to a major highway.
- f. The upgrading of Sepulveda Westway from the Westchester Parkway to La Tijera Boulevard to a secondary highway.

g. The upgrading of Loyola Boulevard to a secondary highway between the north project boundary and the new Westchester Parkway.

28. That prior to recordation, the Department of Airports shall submit evidence satisfactory to the Department of Transportation (DOT) and the Advisory Agency that they have contracted with Commuter Transportation Services (formerly Commuter Computer) or a like organization to prepare, develop and reasonably implement a Transportation System Management (TSM) Program. The implementation of the initial stages of the program will have to be in place no later than the first occupant of any portion of the project (the private contract agreement shall so provide and copies shall be given to the Advisory Agency and Department of Transportation for review).

The TSM Program shall be an integral part of all lease agreements between the Department of Airports and lessees or tenants of the project and should consider those conditions listed in "Q" Condition No. 28, CPC 83-190 (ZC). An integral part of the TSM Program and the contract shall be the designation of a "rideshare coordinator." The rideshare coordinator required for this project, which may be shared by all lessees of the property and with other firms, shall be provided on an continuous

basis for the life of this project. The coordinator shall be an employee of Computer Transportation Services (formerly Commuter Computer) or a like organization. The rideshare coordinator shall submit a transportation system management report for review to the Department of Transportation and the Advisory Agency on an annual basis. The owner shall provide, with the annual report, a copy with a computation of gross floor area, net building area and a description of area use. This information shall also be reviewed to monitor the potential building limitations imposed under CPC 83-190, "Q" Condition No. 6.

In addition to the aforementioned annual report, prior to the recordation of succeeding units after the first tract map unit and three months after the date that any phase or tract unit project is fully constructed and 18 months after each phase project or tract unit is fully occupied, the rideshare coordinator shall report on the effectiveness of the TSM Program and the percentage of the project's employee population participating in the TSM Program. At such time as the entire project is constructed, this evaluation shall be subsumed within the aforementioned annual report.

29. That prior to recordation of each final map unit, the subdivider shall execute an rec-

ord against the property a covenant and agreement in form and substance satisfactory to the City Attorney pursuant to which the applicant shall agree that the owner (s) or lessee (s) or successor (s) in interest of the property involved in this tract map will participate in any benefit assessment district or any trust fund based on a formula or criteria which is applicable to all new developments within the Westchester-Venice-Palms-LAX Transportation Corridor Specific Plan area, had such ordinance, resolution or plan been in effect at the time of approval of this tract map.

The following are among those improvements that should be included in the Specific Plan. In the event the applicant does not participate in such a benefit assessment district or if the benefit assessment district does not provide for the following highway improvements, Items c, h, k and l shall be implemented by and at the expense of the applicant. Any expenses incurred by the applicant for such improvements will be credited to any assessment or fund contribution subsequently placed into effect:

- a. Sepulveda Boulevard between Lincoln Boulevard and north of Manchester Avenue: Widen street and modify median to

provide eight through lanes, plus dual left turn lanes. (Existing right-of-way width is 126 feet south of and 120 feet north of Manchester.)

- b. Manchester Avenue at La Tijera Boulevard:  
As a TSM measure, modify median to provide six through lanes and dual westbound left-turn lanes.
- c. Manchester Avenue at Lincoln Boulevard: Modify median islands and widen to provide six through lanes and dual left turn lanes on all approaches.
- d. La Tijera Boulevard at Sepulveda Boulevard:  
Widen roadway to 80 feet to provide six through lanes and dual left turn lanes.
- e. La Tijera Boulevard at Airport Boulevard: Modify median island to provide six through lanes and dual westbound left turn lanes.
- f. Vista Del Mar at Imperial Highway: Add a northbound right-turn lane, if this is not accomplished as part of the reconstruction of the Hyperion Treatment Plant.
- g. Vista Del Mar at Grand Avenue: Widen to provide left turn lanes.

- h. Lincoln Boulevard between Westchester Parkway and Manchester:  
Remove median island and widen street to provide six through lanes and dual left turn lanes at all signalized intersections in coordination with the Council Office.
- i. Arbor Vitae Street between Airport Boulevard and San Diego Freeway:  
Contribute to the cost of constructing the Arbor Vitae interchange with the San Diego Freeway and associated improvement of Arbor Vitae Street between the San Diego Freeway and Airport Boulevard.
- j. La Tijera Boulevard at San Diego Freeway:  
As an alternative, if the Arbor Vitae interchange is not built, widen La Tijera Boulevard (including the freeway over-crossing) to provide six through lanes and a three lane width median to provide dual left turns onto the freeway (see example LADOT Drawing No. 011,139, Santa Monica Boulevard at San Diego Freeway).
- k. Manchester Avenue at Sepulveda Boulevard:  
As an interim TSM measure, remove median islands and re-stripe all approaches for dual left turn lanes with appropriate traffic signal phasing.
- l. Imperial Highway at Pershing Drive:  
Provide dual westbound right turn lanes.
- 30. That satisfactory arrangements shall be made between the Department of Airports, the Southern California Rapid Transit District and the Department of Transportation to include provisions for bus stops in conjunction with the design of the roadway improvements.
- 31. That the owner reserved a bikeway easement along the extension of Stanmoor Drive and Emerson Avenue or along alternate routes through the tract property to Westchester Parkway to the satisfaction of the Department of Transportation and the Council office.
- 32. That, if determined to be warranted by the Department of Transportation, new traffic signals shall be installed at the intersections of:
  - a. Pershing Drive and Westchester Parkway.
  - b. Falmouth Avenue and Westchester Parkway.
  - c. Loyola Boulevard and Westchester Parkway.
  - d. La Tijera and Lincoln Boulevards.
- e. Lincoln Boulevard northbound on/offramps and Westchester Parkway.
- f. Emerson Avenue and Westchester Parkway.
- g. La Tijera Boulevard and Westchester Parkway.
- h. La Tijera Boulevard and Sepulveda Westway.
- i. Sepulveda Westway, Westchester Parkway and Will Rogers Street.
- j. Westchester Parkway and any major development access driveways.
- 33. That satisfactory arrangements be made with the cable television franchise holder for this area in accordance with policies adopted by the Department of Transportation to assure that cable television facilities will be installed in the same manner as other required improvements. Refer to the Los Angeles Municipal Code, Section 17.05-N. Evidence of the arrangements must be submitted to the Department of Transportation before the condition can be cleared by the Department.

The current cable television holder for this area is:

Jack Barry Cable TV  
6382 Arizona Circle  
Westchester, California 90045  
Telephone: 644-5844

34. That in conjunction with the recordation of any final map units over Lots 3, 5, 6, 8 or 12, the subdivider shall dedicate, as a future street, the land within the tract boundaries necessary to provide for the extension of Lincoln Boulevard underneath the runways to the satisfaction of the Department of Transportation and the City Engineer or that the extension of Lincoln Boulevard be deleted from the Los Angeles International Airport Plan by amendment. The Plan Amendment should, in its preparation, include a study of the feasibility of providing the indicated extension and the impacts to traffic and circulation of its deletion from the Plan.
35. That sufficient land to accommodate a transit station be provided in Lot 11 to the satisfaction of the Department of Transportation.
- S-1 a. That the sewerage facilities charge be deposited prior to recordation of the final map over all the tract in conformance with Section 64.11.2 of the Municipal Code.
- b. That survey boundary monuments be established in the field in a manner satis-

factory to the City Engineer and located within the California Coordinate System prior to recordation of the final map. Any alternative measure approved by the City Engineer would require prior submission of complete field notes in support of the boundary survey.

- c. That satisfactory arrangements be made with both the Water System and the Power System of the Department of Water and Power with respect to water mains, fire hydrants, service connections and public utility easements.
- d. That any necessary sewer, street, drainage and street lighting easements be dedicated. In the event it is necessary to obtain off-site easements by separate instruments, records of the Bureau of Right-of-Way and Land shall verify that such easements have been obtained. The above requirements do not apply to easements of off-site sewers to be provided by the City.
- e. That drainage matters be taken care of satisfactory to the City Engineer.
- f. That satisfactory street, sewer and drainage plans and profiles as required, together with a lot grading plan of the tract and any necessary topography of adjoining areas be submitted to the City Engineer.

- g. That any required slope easements be dedicated by the final map.
- h. That each lot in the tract comply with the width and area requirements of the Zoning Ordinance.
- i. That one foot future street and/or alleys be shown along the outside of incomplete public dedications and across the termini of all dedications abutting un-subdivided property. The one foot dedications on the map shall include a restriction against their use for purposes until such time as they are accepted for public use.
- j. That any one foot future street and/or alley adjoining the tract be dedicated for public use by the tract, or that a suitable resolution of acceptancy be transmitted to the City Council with the final map.
- k. That no public street grade exceed 15%.
- S-2 That the following provisions be accomplished in conformity with the improvements constructed herein:
- a. Survey monuments shall be placed and permanently referenced to the satisfaction of the City Engineer. A set of ap-

proved field notes shall be furnished, or such work shall be suitably guaranteed, except where the setting of boundary monuments requires that other procedures be followed.

- b. Make satisfactory arrangements with the Department of Traffic with respect to street name, warning, regulatory and guide signs.
- c. All grading done on private property outside the tract boundaries in connection with public improvements shall be performed within dedicated slope easements or by grants of satisfactory rights of entry by the affected property owners.
- d. All improvements within public streets, private street, alleys and easements shall be constructed under permit in conformity with plans and specifications approved by the Bureau of Engineering.
- e. Any required bonded sewer fees shall be paid prior to recordation of the final map.

S-3 That the following improvements be either constructed prior to the recording of the map or that such construction be suitably guaranteed:

- a. Construct on-site sewers to serve the tract as determined by the City Engineer.

- b. Construct any necessary drainage facilities.
- c. Install street lighting facilities to serve the tract as required by the Bureau of Street Lighting.
- d. Plant street trees and remove any existing trees within dedicated streets or proposed dedicated streets as required by the Street Tree Division of the Bureau of Street Maintenance. When the City has previously been paid for tree planting, the subdivider or contractor shall notify the Street Tree Division (485-5675) upon completion of construction to expedite tree planting.
- e. Repair or replace any off-grade or broken curb, gutter and sidewalk satisfactory to the City Engineer.
- f. Construct access ramps for the handicapped as required by the City Engineer.
- g. Close any unused driveways satisfactory to the City Engineer.
- h. Abandon sewers in a manner satisfactory to the City Engineer.
- i. Drainage facilities required under Condition No. S-3(b) will include, but not be limited to, the construction of the following:

(1.) Catch basins to the existing storm drains and any new underground drains will be required to provide 50 year protection at the following locations:

- (a.) The proposed intersection of La Tijera Boulevard and Westchester Parkway.
- (b.) Liberator Avenue and 88th Place.
- (c.) Eighty-Eighth Street and McConnell Avenue.
- (d.) The new terminus of Rayford Avenue south of 91st Street.
- (e.) The new terminus of Stanmoor Avenue south of 91st Street.

(2.) Construct storm drains to provide 50 year storm protection at the following locations:

- (a.) Loyola Boulevard and La Tijera Boulevard.
- (b.) The sump in 91st Street at the former Hastings Avenue.

(3.) Provide sufficient storm water collection on Westchester Parkway to keep the storm flow from the traveled lanes for a 10 year storm.



j. Grade the streets being dedicated and adjoining the tract, as required.

k. Improve Lincoln Boulevard being dedicated and adjoining the tract by the construction of the following:

(1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.

(2.) Suitable surfacing to provide a 100 foot roadway which includes a landscaped median island with mountable curbs.

(3.) On- and off-ramps satisfactory to the City Engineer and the Department of Transportation.

(4.) Any necessary removal and re-construction of existing improvements.

(5.) The necessary transitions to join the existing improvements.

l. Improve Westchester Parkway being dedicated by the construction of the following:

(1.) Concrete curbs, concrete gutters and 10 foot concrete side walks with tree wells.

(2.) Suitable surfacing to provide a 100 foot

roadway between Sepulveda Westway and Lincoln Boulevard and a 90 foot roadway between Lincoln Boulevard and Pershing Drive. The roadway widths include landscaped median islands with mountable curbs and bicycle lanes.

(3.) On- and off-ramps at the Lincoln Boulevard overpass satisfactory to the City Engineer and the Department of Transportation.

(4.) Any necessary removal and re-construction of existing improvements.

(5.) The necessary transitions to join the existing improvements.

m. Improve Sepulveda Boulevard by the construction of the following:

(1.) A concrete curb, a concrete gutter and a 10 foot concrete sidewalk with tree wells on the west side between Lincoln Boulevard and 96th Street.

(2.) Place additional concrete to complete a full-width sidewalk on the east side between Sepulveda Eastway and Interceptor Street.

(3.) Suitable surfacing on the west side between Lincoln Boulevard and northerly of 96th Street to provide a 64.5 foot roadway westerly of the center line substantially, as shown on City Engineer's Plan P-26953.

(4.) Remodel the intersection at Lincoln Boulevard to provide a grade separation to accommodate the left turn lanes from northbound Sepulveda Boulevard to westbound Lincoln Boulevard satisfactory to the City Engineer and the Department of Transportation.

(5.) Remodel the median between Sepulveda Eastway and Interceptor Street satisfactory to the City Engineer.

(6.) Any necessary removal and/or re-construction of existing improvements.

(7.) The necessary transitions to join the existing improvements.

n. Improve La Tijera Boulevard between Sepulveda Westway and Westchester Parkway by construction of the following:

(1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.

(2.) Suitable surfacing to provide an 80 foot roadway.

(3.) Any necessary removal and reconstruction of existing improvements.

(4.) The necessary transitions to join the existing improvements.

- o. Improve 88th Street by the construction of the following:
  - (1.) Suitable improvements of the turning area at the westerly terminus of 88th Street.
  - (2.) Suitable improvements at the intersection with the proposed extension of La Tijera Boulevard to provide for the channelization as shown on the tentative map.
- p. Improve Sepulveda Westway by the construction of the following:
  - (1.) A concrete curb, a concrete gutter and a full-width concrete sidewalk with tree wells on the west side between La Tijera Boulevard and Lincoln Boulevard.
  - (2.) Suitable surfacing to join the existing pavement and to complete a 33 foot half roadway.
  - (3.) Remodel the intersection at Lincoln Boulevard to the satisfaction of the City Engineer and the Department of Transportation.
- q. Improve Emerson Avenue from 88th Place to proposed Westchester Parkway by the construction of the following:
  - (1.) Concrete curbs, concrete gutters and 5 foot concrete sidewalks and landscaping of the parkways.
  - (2.) Suitable surfacing to provide 20 foot roadways on both sides of a 26 foot landscaped median island with mountable curbs.
  - (3.) Suitable improvements of the turning area south of 88th Place.
  - (4.) Close Emerson Avenue south of 88th Place to through traffic satisfactory to the City Engineer.
- r. Unless it is deleted as a secondary highway prior to recordation, improve Falmouth Avenue by the construction of the following:
  - (1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.
  - (2.) Suitable surfacing to provide a 33 foot half roadway northerly of 92nd Street and a 66 foot roadway southerly of 92nd Street. If a plan amendment is approved downgrading the secondary highway designation, a lesser roadway will be required.
  - (3.) Any necessary removal and reconstruction of existing improvements.
  - (4.) The necessary transitions to join the existing improvements satisfactory to the City Engineer.
- s. Improve Loyola Boulevard by the construction of the following:
  - (1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.
  - (2.) Suitable surfacing to provide a 66 foot roadway and a 44 foot roadway at its easterly extension to Lincoln Boulevard.
  - (3.) Any necessary removal and reconstruction of existing improvements.
  - (4.) The necessary transitions to join the existing improvements.
- t. Improve the east side of Pershing Drive by the construction of the following:
  - (1.) A concrete curb, a concrete gutter and a 10 foot concrete sidewalk with tree wells.
  - (2.) Suitable surfacing to join the existing pavement to complete a 40 foot half roadway.
  - (3.) Any necessary removal and reconstruction of existing improvements.
  - (4.) The necessary transitions to join the existing improvements.

- u. Improve 92nd Street and Cum Laude Avenue by the construction of an elbow curve within the existing right-of-way, including concrete curbs, gutters and sidewalks satisfactory to the City Engineer.
- v. Improve the terminus of Rayford Drive by the construction of a turning area satisfactory to the City Engineer.
- w. Improve the on- and off-ramps and connector roads for Westchester Parkway and Lincoln Boulevard satisfactory to the City Engineer and the Department of Transportation.